



Grand Forks - East Grand Forks  
**METROPOLITAN  
PLANNING ORGANIZATION**

**TECHNICAL ADVISORY COMMITTEE MEETING**

**WEDNESDAY, APRIL 14<sup>TH</sup>, 2021 – 1:30 P.M.**

**East Grand Forks City Hall Training Room/Zoom**

**PLEASE NOTE:** Due to ongoing public health concerns related to COVID-19, and the fact that the East Grand Forks City Hall is not open to the public; the Grand Forks/East Grand Forks Metropolitan Planning Organization (GF/EGF MPO) is encouraging citizens to provide their comments for public hearing items via e-mail at [info@theforksmpo.org](mailto:info@theforksmpo.org). The comments will be sent to the Technical Advisory Committee members prior to the meeting and will be included in the minutes of the meeting. To ensure your comments are received and distributed prior to the meeting, please submit them by 5:00 p.m. one (1) business day prior to the meeting and reference the agenda item your comments addresses.

**MEMBERS**

Peterson/Kadmas \_\_\_\_\_  
Ellis \_\_\_\_\_  
Bail/Emery \_\_\_\_\_  
Brooks/Halford \_\_\_\_\_  
Riesinger \_\_\_\_\_

Mason/Hopkins \_\_\_\_\_  
Zacher/Johnson \_\_\_\_\_  
Kuharenko/Williams \_\_\_\_\_  
Bergman \_\_\_\_\_

West \_\_\_\_\_  
Magnuson \_\_\_\_\_  
Sanders \_\_\_\_\_  
Christianson \_\_\_\_\_

1. CALL TO ORDER
2. CALL OF ROLL
3. DETERMINATION OF A QUORUM
4. MATTER OF APPROVAL OF THE MINUTES OF THE TECHNICAL ADVISORY COMMITTEE:
  - A. MARCH 10<sup>th</sup>, 2021 MINUTES
  - B. MARCH 19<sup>TH</sup>, 2021 MINUTES

**TECHNICAL ADVISORY COMMITTEE  
APRIL 14<sup>TH</sup>, 2021 MEETING  
PAGE 2**

- 5. MATTER OF PROPOSED AMENDMENT TO FY2021 ANNUAL  
ELEMENT OF 2021-2024 T.I.P. ....HAUGEN
  - A. Public Hearing
  - B. Committee Action
  
- 6. MATTER OF DRAFT MINNESOTA SIDE 2022-2025 T.I.P. ....HAUGEN
  
- 7. MATTER OF DRAFT RFQ FOR PAVEMENT MANAGEMENT  
SYSTEM UPDATE ..... KOUBA
  
- 8. MATTER OF DRAFT AGREEMENT WITH EAST GRAND FORKS  
FOR 5307 FUNDS.....HAUGEN
  
- 9. MATTER OF FUTURE BRIDGE TRAFFIC IMPACT  
STUDY UPDATE ..... HAUGEN/ALLIANT
  
- 10. MATTER OF POSSIBLE EARMARK REQUESTS .....HAUGEN
  
- 11. OTHER BUSINESS
  - A. 2021 Annual Work Program Project Update
    - 1) East Grand Forks Land Use Plan Update
    - 2) Grand Forks Land Use Plan Update
    - 3) Aerial Photo Update
  - B. State Transportation Plan Update
  
- 12. ADJOURNMENT

INDIVIDUALS REQUIRING A SPECIAL ACCOMMODATION TO ALLOW ACCESS OR PARTICIPATION AT THIS MEETING ARE ASKED TO NOTIFY EARL HAUGEN, TITLE VI COORDINATOR, AT (701) 746-2660 OF HIS/HER NEEDS FIVE (5) DAYS PRIOR TO THE MEETING. IN ADDITION, MATERIALS FOR THIS MEETING CAN BE PROVIDED IN ALTERNATIVE FORMATS: LARGE PRINT, BRAILLE, CASSETTE TAPE, OR ON COMPUTER DISK FOR PEOPLE WITH DISABILITIES OR WITH LIMITED ENGLISH PROFICIENCY (LEP) BY CONTACTING THE TITLE VI COORDINATOR AT (701) 746-2660

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021  
Zoom Meeting**

**CALL TO ORDER**

Earl Haugen, Chairman, called the March 10<sup>th</sup>, 2021, meeting of the MPO Technical Advisory Committee to order at 1:33 p.m.

**CALL OF ROLL**

On a Call of Roll the following members were present via Zoom: David Kuharenko, Grand Forks Engineering; Rich Sanders, Polk County Engineer; Jason Peterson, NDDOT-Grand Forks District; Stephanie Halford, Grand Forks Planning; Dale Bergman, Cities Area Transit; Wayne Zacher, NDDOT-Local Government; Ryan Riesinger, Airport Authority; and Jon Mason, MnDOT-District 2.

Absent: Steve Emery, Brad Bail, Ryan Brooks, Jesse Kadrmas, Michael Johnson, Lane Magnuson, Nancy Ellis, Nick West, Lars Christianson, and Patrick Hopkins.

Guest(s) present: Jane Williams, Grand Forks Engineering; Kristen Sperry, FHWA-ND; Stewart Milakovic, NDDOT and Baird Bream, Cambridge Systematics, Inc.

Staff: Earl Haugen, GF/EGF MPO Executive Director; Teri Kouba, GF/EGF MPO Senior Planner; and Peggy McNelis, GF/EGF MPO Office Manager.

**DETERMINATION OF A QUORUM**

Haugen declared a quorum was present.

**MATTER OF APPROVAL OF THE JANUARY 13<sup>TH</sup>, 2021, MINUTES OF THE TECHNICAL ADVISORY COMMITTEE**

***MOVED BY KUHARENKO, SECONDED BY BERGMAN, TO APPROVE THE JANUARY 13<sup>TH</sup>, 2021 MINUTES OF THE TECHNICAL ADVISORY COMMITTEE, AS PRESENTED.***

***MOTION CARRIED UNANIMOUSLY.***

**MATTER OF UPDATE ON NDDOT TRANSPORTATION CONNECTIONS**

Haugen reported that included in the packets was some information on the NDDOT Transportation Connections, or the North Dakota Statewide Long Range Transportation Plan

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

update. He added that the document is available on-line and Mr. Milakovic and Mr. Bream are here today to give a brief update on the progress of the update.

Milakovic commented that Mr. Bream and himself are here today for a very brief update on the plan that they now have available for public comment. He said that they have officially started their public comment period, and Mr. Bream will update some of the provisional information they provided in the reading materials for this month.

Bream referred to a slide presentation (a copy of which is included in the file and available upon request), and stated that he would like to give a very brief update on the release of the Draft Transportation Connection document. He reiterated that they have initiated their public comment period, as required by FHWA and NDDOT Guidelines to receive feedback on the Draft Long Range Transportation Plan. He stated that it is effectively opened now and will remain so through April 12<sup>th</sup> to receive feedback.

Bream reported that they held public meetings yesterday at 2:00 p.m. Central and 6:30 p.m. Central time, and did have good attendance and participation, and they are very pleased with the feedback they received. He added that they are following up with all the registrants via email to send them additional information on the link to the website and the information available there.

Bream commented that if you haven't already done so, please go to their website; [www.transportationconnection.org](http://www.transportationconnection.org) where you will see that the draft document is available in PDF format for review. He added that they also have a feedback survey there as well with questions such as what you thought about the document, where you see areas of improvement, etc., and then they also provide some supported appendices that show some of the activities that they conducted in order to arrive at both the transit scenarios that they identified and the feedback that they solicited from the public and the way they engaged with the public and stakeholders.

Bream stated that for the MPOs they welcome them to submit comments either to your MPO Director, or directly to Mr. Milakovic. He said that if you could please do so before the next meeting for April so that they can have the opportunity to review the comments and respond to them during the meeting.

Haugen commented that, just to clarify the submittal date, by the April MPO meeting date, he is assuming that you would like us to try to get the comments earlier than the actual meeting date. Milakovic responded that they would if possible, adding that he knows that they have talked about potentially holding a meeting with the three MPO Directors in the State to talk about the next steps, in terms of Transportation Connection after it has been adopted; so getting those comments sooner rather than later could definitely aid them in setting up that meeting and setting an agenda, but if it isn't possible please get them to them as soon as you can. He said that April 12<sup>th</sup> is the absolute last date for public comment, but they will work with you as much as they can. Haugen stated that the April Technical Advisory Committee meeting is April 14<sup>th</sup>, and the public comments are due by April 12<sup>th</sup>, so we would normally ask people that want to be on the agenda to get us that information by Friday, April 10<sup>th</sup>, no later than 10:00 a.m. so that it can be

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

included in the agenda packet for the meeting the following Wednesday. He added that anything that would be submitted between then and the 12<sup>th</sup> we would have to discuss on the fly at the Technical Advisory Committee meeting on the 14<sup>th</sup>.

Information only.

**MATTER OF PROPOSED AMENDMENT TO FY2021 ANNUAL ELEMENT OF 2021-2024 T.I.P.**

Haugen reported that included in the staff report there is one modification and then two amendments. He stated that a public meeting notice was published for a public hearing at today's meeting, and that is included in the packet as well, and no written or oral comments were received on the proposed amendment so the action the Technical Advisory Committee takes today is inclusive of any official public comment we received during the public comment period.

Haugen referred to the T.I.P. tables included in the packet and went over the amendments themselves. He stated that the first one is a modification, highlighted in green, that is the result of the Cities Area Transit taking over or absorbing or merging in with the UND Shuttle Service, their actual hours of revenue service increased from 62 ½ to 133 hours so we are reflecting in the T.I.P. that change in hours of service revenue.

Haugen said that the next one is a new project, it is a Statewide project that involves signage to State Parks, and in the MPO study area we do have some signs being installed along the I-29 Corridor, close to the DeMers Avenue Interchange. He pointed out that the dollar amount shown is the amount principally of what is just inside the MPO Study Area. He asked if Mr. Zacher or anyone else wants to elaborate more on this project.

Zacher explained that basically what happened was that the project was created to replace the park signs throughout the State. He stated that these are the big signs and that there were actually two projects, now there are three as they separated the MPOs from Bismarck and Grand Forks into their own project so that they had time to do the different amendments. He added that there is also one for contractors and then another one where the State Maintenance Department is actually going to be making the signs. He said that the larger signs are going to be replaced by contractors.

Haugen reported that the second amendment is following back to our discussion in January and also is attached to the next agenda item, and that is having to show different project bases for T.I.P. projects. He said that this amendment is, as we discussed we would be grouping projects into these different phases for fiscal year, and we would amend the current T.I.P. to reflect just the projects that are being done on the North Dakota side in 2021 that will have federal funds involved in these phases, and so as you see here we have three phases that we have been discussing; they are preliminary engineering (PE), the right of way, and utilities.

Haugen stated that in order to help simplify this T.I.P. management the agreement was that unless a project is deemed to be defined as "regionally significant", which we will get into with

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

the next agenda item, the rest of the project will be grouped and therefore it would be easier for the State, the MPO and the Local Partners to manage the T.I.P. by not having to keep precise track of what the T.I.P. says for each of these individual phases for individual projects, so, as you see here, for Fiscal Year 2021 we do have the separation of the cost estimates by the funding source, there will be some local dollars going towards these projects but the bulk of the funds is the traditional federal with state and local following the formula that is in play for the individual project.

Haugen said that originally he had set this up as, perhaps a natural T.I.P. listing like you would see here, but under the advise and guidance of Mr. Zacher and NDDOT, it is now kind of this stand alone format, where at the end of the Fiscal Year for 2021 we have this group project listing that identifies the projects in this way.

Haugen stated that MPO staff is recommending the Technical Advisory Committee forward a recommendation to the MPO Executive Policy Board that they approve the one modification and also the two amendments, to include these two items. He said that we did ask for public feedback but have received none to-date.

***MOVED BY BERGMAN, SECONDED BY KUHARENKO, TO APPROVE FORWARDING A RECOMMENDATION TO THE MPO EXECUTIVE POLICY BOARD THAT THEY APPROVE THE FY2021 T.I.P. AMENDMENTS, AS PRESENTED.***

***Voting Aye: Peterson, Sanders, Halford, Mason, Zacher, Kuharenko, Bergman, and Riesinger.***

***Voting Nay: None.***

***Abstain: None.***

***Absent: Bail, Brooks, Emery, Christianson, Hopkins, Johnson, Magnuson, West, and Ellis.***

**MATTER OF PROPOSED DEFINITION OF “REGIONALLY SIGNIFICANT” FOR T.I.P. PURPOSES**

Haugen reported that this agenda item plays into what we just did, and that is defining what are our regionally significant projects. He referred to the staff report from January where we introduced this topic to the Technical Advisory Committee. He pointed out that at the very beginning of the staff report it shows an update stating that the NDDOT staff and the other MPOs have been discussing this and sharing back and forth definitions and this is the one that is shaking out so we are now defining three different types of projects that might be regionally significant.

Haugen stated that the first one is similar to the initial draft that the NDDOT had provided, and it pertains to Interstate. He said that the next one is a little re-wording of what had principally been dealing with when they had bi-passes going around the oil country, they wanted to identify those in some way and we have, perhaps, refined that to really just talking about new roadways on new right-of-way that are being somewhat financed with federal funds; and the last one is dealing with Transit, specifically with Transit buildings that are on newly purchased real estate, so that would not include rehabbing on existing buildings, just brand new buildings on

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

new property being purchased, so we are presenting this to this body for review and he did send it to our North Dakota partners previously to get their initial feedback on it, and they asked a couple of questions that he thinks he clarified in his prior discussion about the right-of-way needing to be financed with federal funds, or the new roadway and then Transit has to be new building on newly purchased real estate.

Kuharenko said that he just wanted to make sure that this clarification is; this is kind of all squares are rectangles but not all rectangles are squares, and kind of in a similar nature, the regionally significant projects has a separate definition on a regional project, correct. Haugen responded that that is correct. He explained that until North Dakota changes their grand funding programs and their title, they are still the regional urban program that is separate from our regionally significant projects for the T.I.P.

Kuharenko stated that the other question he has is, he can't remember, was there a previous definition for regionally significant and what was it. Haugen responded that the previous definition for regionally significant included more of an understanding that regionally significant was to catch those projects that were not using federal funds but were significant enough that they had an effect on our travel demand model, essentially; or they were being financed with other federal funds that still required Federal Highway or Federal Transit action on them, and so as we mentioned we are flipping, kind of, away from being those non-federally funded but significant projects to now narrowly focusing in as regionally significant those projects that impact the Interstate or a brand new roadway right-of-way, or transit buildings on new property, all financed with federal funds. He said, however, that the first one may not be financed with federal funds. Kuharenko stated that he was going to say that the one in the first line could kind of throw things off.

Haugen commented that he believes our next step is, as he mentioned, the other two MPOs are having their Technical Advisory Committee meetings as well, so later in March there is a meeting of NDDOT and MPO staff where they will let them know that this is the feedback from our Technical Advisory Committees. He added that NDDOT staff have, also internally, vetted this definition through their processes and they will report back to their internal NDDOT. He said that perhaps, for the first time, we will have Federal Highway actually in the room as we discuss it and he would assume that from that meeting, perhaps in April, we will be taking formal action on a new definition. He added that that will mean that we have to go back and redefine what we had previously defined as regionally significant.

Haugen referred to the staff report and commented that what is highlighted in yellow states that even though we are changing the definition of regionally significant, by and large for most of our T.I.P. projects there isn't much of a change, so a lot of what you see in all of the listings you've seen for decades won't be changing, they will still be processed pretty much the same way they have been. He referred to the tables and went over how the change will be illustrated in the tables.

Zacher commented that a part of this is; it ties in to also making sure that Cities and Counties and MPOs are getting accurate estimates. He said that when you start requesting federal highway

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

funds they need to make sure that they are basing their estimate on something, it isn't just a pie-in-the-sky kind of estimate, so this will mean some additional work on everyone's part. Haugen stated that this is unique to the North Dakota side, however as we define regionally significant it will have impact on the Minnesota side as it is our T.I.P. definition. He added that so far, to-date, discussion with MNDOT has indicated that it would have minimal impact on how we define projects on the Minnesota side. Zacher reiterated that why it has such a minimal impact on Minnesota is because Minnesota doesn't use federal funds for a lot of the intermediate phases, it is used mostly on construction, so all the PE stuff is typically funded with State funds. Haugen said that is correct, adding that the ATP applications are for construction only. Information only.

**MATTER OF ATAC COUNT STUDY AMENDMENT**

Haugen reported that this is something we will ask you take action on. He explained that we have an annual contract with A.T.A.C. for a Traffic Counting Program and periodically we add traffic signals, or we have capabilities because of signal upgrades or other items to engage A.T.A.C. to start counting traffic at new intersections, and that is the case today.

Haugen stated that we have three locations that have been identified; one is where a new signal exists and that is at University and Oxford, on UND campus; the other two have had existing signals for a while and we are now going through, or having learnt the capabilities and are starting a counting program at Columbia and 13<sup>th</sup> and 4<sup>th</sup> Avenue and Cherry.

Haugen said that the amendment is to our existing contract for the traffic counting, it is for A.T.A.C. to set up the three intersections just identified, and get them included in our reporting program so that we can start accessing counts at these locations. He added that the contract is just under \$5,000 and is within the budget that we have for this activity, so staff is recommending that the Technical Advisory Committee approve forwarding a recommendation to the MPO Executive Policy Board that they approve the scope of work for the A.T.A.C. Traffic Counting Program.

***MOVED BY KUHARNEKO, SECONDED BY SANDERS, TO APPROVE FORWARDING A RECOMMENDATION TO THE MPO EXECUTIVE POLICY BOARD THAT THEY APPROVE THE DRAFT SCOPE OF WORK FOR THE A.T.A.C. TRAFFIC COUNTING PROGRAM, AS PRESENTED.***

***Voting Aye: Peterson, Sanders, Halford, Mason, Zacher, Kuharenko, Bergman, and Riesinger.***

***Voting Nay: None.***

***Abstain: None.***

***Absent: Bail, Brooks, Emery, Christianson, Hopkins, Johnson, Magnuson, West, and Ellis.***

Haugen commented that there are a couple more traffic signals that the City is installing; the anticipation is that they would not be installed and in place yet this construction season, so that is why we aren't including signals at say South Washington and 36<sup>th</sup> Avenue or 47<sup>th</sup> Avenue and



**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

20<sup>th</sup> Street. He said that they also have an issue that, if you notice, this is an amendment to a Master Agreement, that Master Agreement expires in the fall of this year so we are trying to keep projects from crossing over into a new Master Agreement, so that is another reason why we did not include some of those signals that are on order but not installed yet, but we are aware of them and will work to get them included once we can.

**MATTER OF APPROVAL OF MEMORANDUM OF AGREEMENT**

Haugen reported that a Memorandum of Agreement is a federal requirement that the MPO, State DOT and Transit Operator, and in our case it is two State DOTs and two Transit Operators that need to reach some sort of agreement that identifies with the 3-C Planning and Programming process.

Haugen stated that this is not a new requirement, it has been a long-standing requirement of federal law. He added that he is aware of the 1994 Memorandum of Understanding that is basically the framework that the 2010 Memorandum of Agreement, the existing one, and then this new revised agreement is based on that framework, so again this is a long-standing item.

Haugen commented that the other thing we are changing, not that it means a lot, is that we have gone away from Memorandum of Understanding to Memorandum of Agreement, and essentially the language is coinciding more with the current FAST-ACT requirements; since 2010 we have had a couple of federal re-authorization acts so we are updating language to reflect that, but we are also now adding something that has been a common expression when we talk about the memorandums, that they aren't really worth the paper they are written on so this actual MOA does have further understanding that the MOU is not a legally binding agreement and creates no legally bonding obligations and for any part, so it is actually putting it in black and white and as part of the agreement.

Haugen stated that, as he mentioned, they have been exchanging drafts for a couple of years now, and we think that both DOTs are fine with the agreement and will be willing to sign it after both the Transit Operators and the MPO sign it. He commented that both Transit Operators have approved the Memorandum of Agreement and are willing to sign it, so it is now the MPOs turn and staff is recommending approval of the Memorandum of Agreement as presented. He pointed out that included in the packet were the staff reports that were submitted by both City Transit Operators to their respective City Councils, and then the full agreement itself and all of its permissions.

Zacher said that, just as an update on where they are at with the signature process; the way he set it up for getting all of the signatures was so that Ms. Ellis and Mr. Bergman would get notice when it goes to their respective Cities, and Mr. Haugen would get notice when it goes to the MPO. He stated that currently it is at East Grand Forks, so he has it set up to go from East Grand Forks to Grand Forks, and then to the MPO and then it will go to MnDOT, and then ultimately back to the NDDOT for signatures, so it is at the Mayor of East Grand Forks' office currently and Ms. Ellis received her copy so she is aware of that, and then Mr. Bergman should be getting an email as soon as East Grand Forks has signed. He said that he included the Transit Operators

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

and the MPO just so that people would be aware of where it is at so they can follow up with the person signing the document to ensure it is getting signed.

Haugen referred to the agreement, Item #9, Performance Based Planning, and explained that there is another agreement that spells out responsibilities specific to performance base. He said that that is an agreement that we have, as the Technical Advisory Committee, acted on in the past, and he is sure that it is an agreement that Mr. Zacher will be working on to update on the North Dakota side as soon as this MOA is off his plate.

Bergman commented that he did let the Mayor know already so he is waiting for the signature page to come through to him. Zacher reiterated that it will go to Grand Forks once East Grand Forks has signed it.

Mason asked if this agreement had been reviewed by Anna Pierce. Zacher responded that it had. He added that Ms. Pierce was made aware of it and he included her on the e-mail that went out yesterday with a note that she will receive a copy when it is MnDOT's turn to sign the document. Haugen explained that Bobbi Retzlaff, Ms. Pierce's predecessor was involved, and as he said this has been going on for a couple of years, unfortunately, for justified reasons, but we are at the conclusion, so MnDOT has been at the table each and every time.

***MOVED BY SANDERS, SECONDED BY PETERSON, TO APPROVE FORWARDING A RECOMMENDATION TO THE MPO EXECUTIVE POLICY BOARD THAT THEY APPROVE AUTHORIZING THE MPO CHAIRPERSON SIGN THE MEMORANDUM OF AGREEMENT, AS PRESENTED.***

***Voting Aye: Peterson, Sanders, Halford, Mason, Zacher, Kuharenko, Bergman, and Riesinger.***

***Voting Nay: None.***

***Abstain: None.***

***Absent: Bail, Brooks, Emery, Christianson, Hopkins, Johnson, Magnuson, West, and Ellis.***

**MATTER OF FY 2020 ANNUAL LISTING OF OBLIGATIONS**

Haugen reported that each year we are required to do this annual listing of obligations. He explained that it is a way for us to check our financial plan to see how it is progressing, as well as to check on the status of projects. He stated that we are also required to specifically identify if there has been any bike or ped facilities as part of a project.

Haugen said that, if you recall, last year was the first year that we separated our annual listing of obligations as being part of a T.I.P. document as an appendices; we were running into confusion between project status, and also a listing of obligations, and then we were also being further confused by having a year so close to a year when the active obligations were still taking place, so we have separated them out. He stated that last year we adopted a document that shows our 2019 obligations, and this year we are looking at 2020 obligations. He added that that doesn't

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

mean that in the T.I.P. document we still won't be asking you about your project status for 2021, that will still be an appendices in the T.I.P. document, but this document itself is only focusing on the 2020 projects, and hopefully all of the verbiage, if you read through it, said the same thing. He added that the important thing is to compare our T.I.P. programmed cost estimates versus what was actually obligated.

Haugen pointed out that the first couple of pages are all transit related; the very first project is transit operations, and we know that CARES ACT, the Covid 19 pandemic really threw a monkey wrench into program versus obligation CARES funding so it was a challenge for the Transit Operators to identify how things were or are, so you will see that for Grand Forks they were receiving about three times what their annual 5307 program appropriation is, and to-date Mr. Bergman has indicated that they have spent about \$1,000,000 of it so the total cost and such are still waiting for a year-end report, but there was still the normal programmed projects that Covid didn't affect as much, and so where there is still a requirement to spend 1% on safety and security, that shows the cost estimate program versus obligations highlighted in yellow for three capital purchases and you can see that there were obligations made in all three of them.

Haugen commented that for the first one there is still a part of the project that has to be done, and that involves some shelters along University Avenue, which are in the que to be done; and then we have our traditional street projects, where we have, again, a comparison again of basically what the estimate was when we programmed it many years ago versus what the actual obligations were for that project, and then Transit usually doesn't have bike/ped facilities, but on the street side there is opportunity and we had noted what we could on how the project improved bike/ped facilities.

Haugen stated that we have the North Dakota listings, just as we do in the T.I.P.; and we have the Minnesota listings on the Minnesota side that are all related to transit service, there were no street projects on the Minnesota side in 2020. He added that, again, on the Minnesota side Covid-19 drastically changed costs and also the availability of federal funds, and you will see that reflected in their operational points of view on each one of these, and also in our actual 2020 T.I.P. year we actually had a project for them to get funds for their 2019 appropriation so that is what this last project ends up showing.

Haugen said that at the end we have basically a summary. He explained that they were trying to program \$38,000,000 in total projects; we actually obligated \$25,000,000. He said that two projects were delayed on the North Dakota side, and they were substantial with the biggest one being the 32<sup>nd</sup> Avenue HSIP or safety project, it has been awarded, but it shows up in FY2021 so we showed it as being delayed from FY2020. He stated that again, with Covid, they are showing that it makes it difficult to compare for the transit side what was actually programmed versus what was actually been able to be obligated, and as you can tell there is substantially more CARES funds still available from the initial CARES funding and there may be more coming down the pike if the House takes action today on the \$1.9 billion package, so we still have a lot of regular 5307 funds to utilize that still have one plus three years to be obligated.

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

Haugen stated that this is the Draft Annual Listing of Obligations Report that we prepared and we would seek comments or questions on it. He added that staff is recommending that we have this adopted by the MPO Executive Policy Board at their next meeting.

Bergman commented that he just noticed the one issue with the hours service and stuff for the East Grand Forks side. He said that he didn't get a chance to talk to Ms. Ellis about it; did you happen to talk to her. Haugen responded that he did not talk to her about the hours. Bergman stated that that would be the only question that he would have, to make sure that that is correct. Haugen responded that that would be more effective to our next T.I.P. document, but he will make a note of that because we will be drafting that soon.

***MOVED BY BERGMAN, SECONDED BY PETERSON, TO APPROVE FORWARDING A RECOMMENDATION TO THE MPO EXECUTIVE POLICY BOARD THAT THEY APPROVE THE FY2020 ANNUAL LISTING OF OBLIGATIONS, AS PRESENTED.***

***Voting Aye: Peterson, Sanders, Halford, Mason, Zacher, Kuharenko, Bergman, and Riesinger.***

***Voting Nay: None.***

***Abstain: None.***

***Absent: Bail, Brooks, Emery, Christianson, Hopkins, Johnson, Magnuson, West, and Ellis.***

**OTHER BUSINESS**

a. 2021 Annual Work Program Project Update

Haugen reported that this is our monthly summary of where we are at with our work program projects. He stated that, you will notice that we will discuss the East Grand Forks Land Use Plan Update, the Grand Forks Land Use Plan Update, The Future Bridge Traffic Impact Study, and the Aerial Photo Update projects individually. He added that also in the monthly summary sheet we have the updates there as well.

b. East Grand Forks Land Use Plan Update

Kouba reported that they did hold the first public meeting on February 22<sup>nd</sup>, and at that point they also started a survey and continued with their Wiki Map Application for input. She said that as of right now they have about 70 responses to that survey, and other than that they are still trying to get the word out about the survey, which is open until March 15<sup>th</sup>, so we can then move on to the next step.

Kouba stated that they are asking the City of East Grand Forks for their help in getting the word out about the survey, and they may be doing some in-person type of applications where they will go out and talk to people where they can.

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

Haugen commented that he meant to include the link for the website that has been set up for this study, it is: [www.egfplan.org](http://www.egfplan.org). He said that a lot of this information is available there, or you can find it on our website as well, and it is: [www.theforksmmpo.org](http://www.theforksmmpo.org).

Kuharenko said that, since we are getting into both the Grand Forks and East Grand Forks Land Use Plan; just the update on the Long Range Transportation Plan in general, he seems to remember that a while back there was a one page sheet that kind of showed the timeline that the Land Use Plans were going to be completed, the Transit Plan, the Bike/Ped Plan; could we start seeing those again, a one sheet summary in the packet to give us a better idea of what the overall timeframe is and then what is the end date that we can't go past as well, then if the schedule would end up shifting or moving we would get a better idea of how that is going to impact the other modules that we have to complete, is that something we can get added to the packet. Haugen responded that it is already available to you in our Work Program, if you just click on our Work Program it is there. Kuharenko said that he is saying in addition to the Technical Advisory Committee packet, if we could see that on a monthly basis, just add it in there as a reference. Haugen responded that we can cut and paste that page from the Work Program, but it is there and available so you don't have to wait for us to put it in the packet.

Information only.

c. Grand Forks Land Use Plan Update

Haugen reported that, again, they are about 6 to 8 months behind East Grand Forks, and the first Land Use Subcommittee meeting was held this past Monday, and the website is now live, the link is: [www.gf2050plan.com](http://www.gf2050plan.com).

Haugen commented that on that website you do have the meeting materials that were discussed at the Monday morning meeting with the Land Use Subcommittee. He stated that this project is just getting off so that meeting was sort of an initial introduction of the consultant, SRF and Praxis, to the Land Use Subcommittee and also several City Departments. He explained that one of the activities was to start working on the Goal Statements that are in the 2045 Land Use Plan; again, if anyone from the Technical Advisory Committee is interested that information is on the [www.gf2050plan.com](http://www.gf2050plan.com) website. He stated that there will be more meetings and materials coming out soon; it is going to be active for the Grand Forks Land Use Plan, from this point forward.

Halford asked since Grand Forks delayed starting their Land Use Plan so East Grand Forks got a head start; is there anything out of that plan that is going to conflict with what comes out of our plan because theirs will be approved before ours will and she is just wondering if there is going to be something that comes out of their plan that might be a concern on our side that maybe we wouldn't want in there. Haugen responded that that is kind of a question that is not dependent on timing but more on content, and that question would probably better if it were timed to be in the same month, so the answer would be to participate, be aware of, and let staff know if you have concerns of what content is being developed, and this goes for all of the Technical Advisory Committee members on the documents.

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

Kuharenko commented that he knows that an e-mail was sent out this morning discussing a special Technical Advisory Committee meeting next week. Haugen responded that that is what he was just going to get at with the next item.

Information only.

d. Future Bridge Traffic Impact

Haugen reported that, as you are aware, Alliant was selected and are on board to do this study, and they have started in earnest and one of the things that the MPO Board asked the Technical Advisory Committee for feedback on, and that has to do with the Ad Hoc Group. He stated that we have since gotten further guidance from the MPO Executive Policy Board at its February meeting and that group has been formed and so it is established and ready to meet, so they are trying to schedule that meeting in early April. He added that the website and existing conditions report are still being created, but the reason for the e-mail this morning asking for a Special Technical Advisory Committee meeting is because we would like to have the Technical Advisory Committee work as sort of the project management team on this study as we work with Alliant on identifying who might be the best choice to be on the management team. He said that it came to look a lot like our Technical Advisory Committee membership, and the Technical Advisory Committee is already regularly meeting so after this request for a special meeting, we are working on having it as an agenda item on our regular Technical Advisory Committee meetings because we do have a completion date at the end of this year, so that means for many months in a row we will be having to have technical guidance from members of the Technical Advisory Committee.

Zacher asked if he was omitted from that e-mail, he doesn't remember seeing anything on that. Haugen responded that it was at the very top half of the e-mail that has this zoom meeting information, in larger bold letters. Kuharenko added that it looks like Mr. Zacher was included, it came in at 8:11 this morning. Zacher said that he just didn't see it.

Haugen asked what everyone's availability was for either next Wednesday afternoon or Friday morning. He said he wouldn't anticipate it will take over an hour. Halford asked if he was asking for an answer now or will he be sending out a Doodle Poll for times. Haugen responded that he was hoping to be able to avoid a poll, so maybe everyone could look at their calendars and let us know what will work. After some discussion it appeared that Friday morning would work best for most members, around 8:30 a.m..

Information only.

e. Aerial Photo update

Kouba reported that there isn't much to discuss beyond what is included in the activity sheet. She stated that we did sign the contract and are currently working on figuring out when Ayres

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 10<sup>th</sup>, 2021**

can actually do the flying for the imagery. She added that we are pretty clear right now so hopefully it will be done sooner than later.

Information only.

Haugen referred to the project activity sheet and commented that you can see the other two projects in the work program; Pavement Management and the TDP Update have had little activity to-date as we have been busy with these other items, but we will get them both up and going soon.

**ADJOURNMENT**

***MOVED BY BERGMAN, SECONDED BY SANDERS, TO ADJOURN THE MARCH 10<sup>TH</sup>,  
TECHNICAL ADVISORY COMMITTEE MEETING AT 2:33 P.M.***

***MOTION CARRIED UNANIMOUSLY.***

Respectfully submitted by,

Peggy McNelis, Office Manager

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE SPECIAL MEETING  
Friday, March 19<sup>th</sup>, 2021  
Zoom Meeting**

**CALL TO ORDER**

Earl Haugen, Chairman, called the March 19<sup>th</sup>, 2021, special meeting of the MPO Technical Advisory :Committee to order at 8:33 p.m.

**CALL OF ROLL**

On a Call of Roll the following members were present via Zoom: Nick West, Grand Forks County Engineer; Nancy Ellis, East Grand Forks Planning; Steve Emery; East Grand Forks Engineering; David Kuharenko, Grand Forks Engineering; Rich Sanders, Polk County Engineer; Jason Peterson, NDDOT-Grand Forks District; Dale Bergman, Cities Area Transit; Wayne Zacher, NDDOT-Local Government; and Jon Mason, MnDOT-District 2.

Absent: Brad Bail, Ryan Brooks, Stephanie Halford, Jesse Kadrmas, Michael Johnson, Lane Magnuson, Lars Christianson, and Patrick Hopkins.

Guest(s) present: Anna Pierce, MnDOT-St. Paul, Kristen Sperry, FHWA-ND; Tim Burkhardt, Alliant Engineering.

Staff: Earl Haugen, GF/EGF MPO Executive Director; Teri Kouba, GF/EGF MPO Senior Planner; and Peggy McNelis, GF/EGF MPO Office Manager.

**DETERMINATION OF A QUORUM**

Haugen declared a quorum was present.

**MATTER OF FUTURE BRIDGE TRAFFIC IMPACT STUDY UPDATE**

Haugen reported that we are just getting underway with the Future Bridge Traffic Impact Study and we did go through an RFP process and hired Alliant Engineering. He stated that the purpose of this meeting today is to establish the Technical Advisory Committee as our Project Management Team to be a sort of sounding board for the consultant and staff as we prepare materials to release to the Ad Hoc Group, the public, our respective agencies, and ultimately to the MPO Executive Policy Board on the study process.

Haugen introduced Tim Burkhardt, Project Team Lead for Alliant Engineering, who was present for a brief presentation on how the study process will take place, and give an update on where they are at on it.



**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 19<sup>th</sup>, 2021**

Burkhardt referred to a slide presentation (a copy of which is included in the file and available upon request). He thanked everyone for attending the meeting and explained that his goal is to really get to know you a little bit and have you get to know him for the purpose of working together on the study and providing regular updates and making sure he knows where you are coming from; if there are particular issues, questions, hope for outcomes that he doesn't seem to be capturing, he definitely wants to hear that whether today or at subsequent meetings.

Burkhardt stated that he will go through high and/or medium levels on what the study is, the work that we will be doing, and he will go through it fairly quickly and then we can come back if you have specific questions.

Burkhardt went over the agenda for the project, stating that they started work on the project in February, so they are almost six or so weeks in. He referred to the next couple of slides and introduced the project team.

Burkhardt commented that they are using a 3-step approach, sort of an approach to a lot of planning studies whereby he likes to divide it out and right now we are in Phase 1, which he likes to call "Discovery". He explained that with this phase they are learning about things that we knew or didn't know, learning more about traffic operations in particular right now, specifically what we call existing and future conditions which help he will dive into more deeply. He said that this is the foundation for the project where they will look at traffic paths, to make sure they get that all right and that they understand it before they go forth and develop options for river crossings and impacts on the whole system as it relates to a new link in the transportation system.

Burkhardt stated that the next phase is "Development" which is where they will develop and evaluate the alternatives and concepts in more detail and understand how they perform.

Burkhardt said that the last but not least phase is "Documentation" is to build a report, but also the implementation plan will help provide insight and direction as to, okay we've got the conclusion to this study but how do we take it to the next step, whether that is construction phasing or funding or triggers for the NEPA process, etc.

Burkhardt referred to a slide with the Schedule Overview and went over it briefly, pointing out that March is highlighted. He commented that you will also see three little red blobs in the public involvement boxes, which are three public input events, that he will talk about more as well.

Burkhardt referred to the next slide, which discusses Task Status and First Deliverables, and went over it. He stated that he will provide this table each time we meet to just to give you a high level of what they are doing, what they accomplished, and what they are working on. He pointed out that you can see that the first four tasks are underway.

Burkhardt referred to the next slide, Engagement Approach and Tactics, and pointed out that again you will see the three project phases. He stated that the third column gives us further detail of things like what are we going to do to engage the public; things like an interactive comment

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 19<sup>th</sup>, 2021**

map and survey, and that is the backbone and is a really good application for this project in that it will provide a map where people can comment; the Ad Hoc Group meetings that he thinks are a really critical part of getting engagement on the project to a cross section of the public and others; public input meeting, and pop-up activities.

Burkhardt commented that some of these next slides he can go through pretty quickly, it is really just acknowledging that, when he thinks about doing this study, who cares about it, and how do we engage them with both agencies staff, which includes all of you and others, and with the public who have sort of overlapping but different interests and priorities, and it should do a good job of engaging both of them and then try to bring those perspectives together if need be, and again the last is the Ad Hoc Group, which hopefully is one of our strategies to integrate the different perspectives as we go along. He stated that part of this is managing and thinking about what information we put out to the public, in particular what are our messages, what are we asking them for input on, just trying to keep that goal focused and thoughtful so it isn't just a scattered shot, so it resonates with people.

Burkhardt referred to the next slide, which is an example of what the social pinpoint map looks like, this one is a mock-up showing what it will look like when we produce this, which they are just setting up in the next week or two. He explained how the map will work and commented that it is one of his favorite tools as it is really easy and is mobile friendly.

Burkhardt referred to the next slide illustrating a pyramid and commented that there is a lot going on here, but from his perspective it is an important tool that describes how he intends, with your help, to make this project work from an engagement standpoint. He stated that looking at the decision structure at the top, one thing he likes to try to be clear about when we involve this board, agency staff, the public is, what are we asking of you, what are you going to do, what are we going to do, and determining who is making the decisions is one way to get at that, so as you will see on the right hand side it indicates that ultimately it is the MPO Executive Policy Board that will make decisions on this study; next the Technical Advisory Committee will make recommendations, taking input and making recommendations to the board; next is the Ad Hoc Group, which we will talk about the composition of, and they are very much intended to understand where the public and others are coming from and to take that input and make recommendations to the Technical Advisory Committee; the General Public will provide input to the Technical Advisory Committee; and at the bottom is the consultant team and the MPO staff will manage and conduct the study.

Burkhardt stated that the Ad Hoc Group is intended to provide balanced representation, which in his experience is a great tool when you have lots of perspectives, and in this case having two sides of the river sets it up well for controversy in terms of, we want this we want that this works for us this doesn't work for us; so rather than fall into that trap of if it's there, the Ad Hoc Group, the intention is to think about what are the issues and interests that should be represented when we talk about this issue, the river crossing; and then try to bring together a group that brings that together. He said that you will see a list of the members coming up in the presentation.

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 19<sup>th</sup>, 2021**

Burkhardt commented that in terms of, probably not different from other committees, but he tries to be clear about the expectations, if you are on the group we want you to attend, to engage and connect with the public, and have your ear to the ground as to what the broader community issues and conversation is, and then working for consensus rather than voting; and when it comes down to a decision or recommendation try to work it out before making a decision or recommendation.

Burkhardt referred to the next slide and commented that it shows the three key decisions that we will want to make, ultimately what we represented and the technical reports we produce; we will be asking for input from the public on these as well until we get to that conclusion; so the foundation of purpose and need, why are we doing this, what is the purpose of an additional river crossings, what do we hope it will accomplish for transportation. He added that purpose and need is somewhat already set by the work that has been done and documented in the Long Range Transportation Plan from prior studies, but we will put a bit more meat on the bones of that document. He said that the second decision is the river crossing alternative, and on one hand the MPO is focused on two corridors, as well as a no-build option; so there is the 32<sup>nd</sup> Avenue location and the Elks Drive location, so on one hand the alternatives that we are looking at in the study have been defined, but obviously the hard work will be, what does that really mean, if we put the connection at 32<sup>nd</sup> Avenue what other impacts does that have on the transportation system, in terms of improving that intersection. He stated that the third one is recommendation, and this is where we come back to this language of “what is the conclusion of this study”, hopefully it is clarity on these two corridors that one clearly works better, meets the purpose and need better; expectation is that we will document that in the study. He said that as our FHWA and DOT staff know, at this phase of a study what we don’t want to do is to use the language that we have a “preferred” alternative because that gets us in trouble in the NEPA process.

Burkhardt reported that the next two slides show a list of the Ad Hoc Committee “Seats”, but they are actually geographies or issues or organizations that we want represented on the committee. He stated that Mr. Haugen and himself talked about who/what should be represented, and vetted it with others, to come up with a list of eleven representative seats. He referred to the next slide and commented that this one shows the names of the representatives on the Ad Hoc Committee. He stated that the first Ad Hoc Committee meeting is scheduled for the morning of April 6<sup>th</sup>.

Haugen wanted to emphasize that this group will meet for the first time, and he is wondering if there is another slide that highlights the time and place for this meeting, or should we get into that little detail now. Burkhardt responded that he does not have a slide that shows the time and location, so the first meeting will be held on Tuesday, April 6 at 9:30 a.m. in the Grand Forks City Hall Council Chambers. Haugen added that he did send an e-mail to the Technical Advisory Committee, most of the people on this meeting, so they should have that detail, but just wanted to highlight that we do have all 11 members committed to meeting that Tuesday morning. He said that it will be a hybrid type meeting where some people will be available in person but the majority, perhaps most of the people on this Zoom call, will be participating via a Zoom Webinar and he will get that information out. He added that we still have to practice physical distancing and that does limit space within the room; we did get the largest room we

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 19<sup>th</sup>, 2021**

could that still allowed webinar technology plus met all of our ADA requirements, etc., but it is still limited to an actual number of people that can be in-person, so we would ask that most Technical Advisory Committee members please participate via the Zoom webinar. Zacher asked for the date and time of the meeting again as he doesn't have it on his calendar. Haugen reiterated that the meeting is Tuesday, April 6<sup>th</sup> at 9:30 a.m. in the Grand Forks City Hall Council Chambers. Haugen responded that an e-mail was sent but a calendar invitation hasn't been sent yet. Kuharenko commented that he was going to mention that he hasn't seen a Zoom invitation yet. Haugen responded that he hasn't sent a Zoom Webinar invite yet, but you should have received the e-mail identifying the date, time, location, participation type, etc. Burkhardt asked if they should send those save the date invites to hold that time on people's calendars. Haugen responded that they can. He added that part of this is that they are working through Public Info at the City of Grand Fork and understanding the possibilities that we have with all of the technology available, and so the full Public Info staff was not available until next week, and that is why you got just a heads-up e-mail and not a full invite at this time as there are still some details being worked on. Burkhardt said that while we are on this topic the Ad Hoc Group is intending to meet a total of five times, so it isn't a monthly meeting, so they will meet at key milestones, so five times between April being the first one and before the end of the year, but he will be holding a monthly time for that, but it will be more like every other month.

Bergman asked if there would be any general public individuals on these committees; you see community and business, but most of the individuals he sees on the list are business owners, so that is why he is kind of curious because that is where you are getting the pushback from on anything, from the general public. Haugen responded that obviously we have two elected officials, and then community numbers three through eight were all selected by a combination of the neighborhood, working with their Alderman (Ward Person), and that is how they were selected so, again, for seats number three through eight are representing that neighborhood and they were selected through a neighborhood selection process, and that selection process was pretty much driven by the councilmembers whose wards they are in, and so he doesn't have all the details of exactly how it was done, but he does know for the Near Southside and 32<sup>nd</sup> Avenue neighborhoods their recruitment was through several active Facebook Group Pages that were available and other social media means, so he does believe that even though you may know these people because where they work, that isn't how they were selected, they were selected to represent the seats that we identified, they were selected through the process that was used in that individual neighborhood.

Burkhardt referred to the next two slides, Public Involvement Plan, and stated that the point of including them is just to show that, they, as a consultant team really outline what each of the meetings is about, whether it is a public, Technical Advisory Committee, or Ad Hoc Group meeting. He said that the reason for this is for them to be able to think through what they are going to do at the meeting, what are we trying to accomplish, and certainly for the public events how are we advertising it and what are the outcomes, etc., so this is a document that they share with Mr. Haugen in order to ensure we are all on the same page, and then for the Team to use as we go forward. He just wanted you to know that it exists and it is available if you are interested in digging into the details.

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 19<sup>th</sup>, 2021**

Burkhardt commented that the next slide is just a work in progress map; as we get into communicating with the public we will be preparing graphics to help people understand all kinds of things, so for now he just wanted to share the proposed alignments in relation to the two cities for bridges, nothing more on there, but there is a lot more that will go on various maps.

Burkhardt stated that we are not coming up on traffic, which is kind of what this is all about, or mission of what we are trying to accomplish and understand. He referred to the next slide, Traffic: Establishing a Baseline, and explained that it really goes through steps that will be familiar to all of you who are in engineering and traffic planning, so he will go through it quickly and just kind of recap the process.

Burkhardt said, starting with understanding existing traffic volumes, establishing that baseline, you will notice the word Covid on here as at this point, for better or for worse we are all pretty familiar with the fact that traffic in the last year is not representative of the recent past or what we expect in the future, so our traffic counts, if we are doing new traffic counts, are adjusted for this.

Burkhardt commented that the traffic volumes; you know intersections, obviously the key issue always, and if we anticipate changes in the network, that will continue to be the case.

Burkhardt stated that safety, again, can relate to traffic volume or not, but it is part of our existing understanding, are there issues that should be addressed, are there issues that were exasperated by the changes that we might purpose.

Burkhardt referred to the Key Intersections and Traffic Counts slide and commented that it lists the intersections that they are examining in detail with traffic counts. He pointed out that in the right-hand column if you see Alliant indicated that was done by their staff when they were here about two weeks ago collecting traffic counts at intersections where we didn't have recent counts from other sources, and they are not working through that data.

Burkhardt stated that also part of establishing that baseline they will be using Streetlight, which a number of you are familiar with, it is an amazing tool that has kind of exploded their ability to understand origin destination, where people are going to and from, using data collected by a smart phone, so they will be doing Streetlight analysis and they are working right now with A.T.A.C. who is collecting that data for them via their license, and then we are putting it together and documenting it and will be displaying it. He added that on the local system, street system, it will help show where people are traveling to and from right now and how does it relate to demand for an additional bridge crossing.

Burkhardt commented that, as he said at the beginning, a lot of this has been looked at before, and there is existing information that they are making use of, updating it obviously and bringing in their own expertise to that, but some of this not brand new information, but hopefully the way they display it and communicate it and use it, is slightly indignant to these two corridors, that should be a new and valuable contribution.

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 19<sup>th</sup>, 2021**

Burkhardt stated that as you know both Cities are updating their Land Use Plans right now, and that is important for us as we look to the future, where do we think traffic is going to be in the future, to make sure it reflects where we think the land use is heading, so we are doing that coordination right now as well, and again are working with A.T.A.C. to develop and make any adjustments needed to the traffic forecast they have already done based on some changes in land use that are anticipated as part of the plan update.

Burkhardt said that lots of performance measures that they will use, which you are familiar with; whether it is at intersections, on segments, probably that safety, multi-modal needs like bicycling, pedestrian network and features, certainly anywhere we purpose changes they will be looking for any adverse impact, unintended consequences, and then on the positive side looking to enhance the ability to walk and cycle in the community as is consistent with other plans, certainly that is a base assumption of a new river crossing, that it will include good bicycle and pedestrian facilities at a crossing.

Burkhardt stated that, again, sort of the process, that after we get all the information, then the question is still what; what are the needs, what do they tell us, and how does that translate into a solution, so, again, we build a connection across the river at one of these two location, how it is going to change traffic flow, is it going to bring more traffic in some locations and less in others, and then it is up to us to figure out how to address an issue that raises at an intersection, another key example.

Burkhardt said that communication flows throughout our effort on the project, whether it is directly in the public events, but also just sharing what we have learned; what the issues are, what opportunities are, so the traffic again, things like clear color-coded maps that are easy to read, not too busy and then you can see the videos operating on the right, is a really nice tool.

Burkhardt stated that he has one graphic here, it is more important than one, but as they develop and flesh out the crossing options and what that means to the intersections, on 32<sup>nd</sup> Avenue, developing easy to understand and transparent, in terms of impacts, graphics is really important; one of them is near to the top image which is a concept layout over an aerial. He added that the other thing which he doesn't have today, but that is really a nice feature is overlaying a project drawing like this on top of Google Earth, which some of you may have seen, and then you can easily animate it and it shows you more of a 3d look, which he thinks is helpful for us as professionals, but also for members of the public.

Burkhardt commented that the next slide is something he hinted at earlier, in terms of the environmental process. He said that this river crossing study is not an environmental document, it comes before the formal NEPA process, but there is something called the PEL process – Planning and Environmental Linkages, that a number of you are familiar with. He stated that his translation of what that means is doing a good planning study, it is not getting ahead of yourself, in terms of the NEPA process, and this slide has a few rules to live by, to actually keep yourself out of trouble. He added that, whether it is a study like this one or others, the transportation planning library is littered with examples of studies that go too far, and then the project advances into the Environmental process, which then says you have to go back and do that alternative

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 19<sup>th</sup>, 2021**

analysis over again because you didn't do it in a way that complies with the federal environmental process. He said that what that means to him is the purpose of having a purpose, which we do and will, using that to evaluate options, to have a basis for the technical analysis that we do and then the conclusions that we draw, and involving the public in all of that in the purpose and need, and in the evaluation process and documenting how the public was involved, and then what's important at the end, is not coming to a conclusion that says we looked at everything, or we looked at these things, and this is our recommended alternative, and we are done and this one is moving forward; you can think that, we can have one that clearly meets our evaluation criteria better than others, but we need to keep the door open to flow into a future NEPA process, so he would be glad for any input from those of you who have your additional thoughts and opinions on that, but he just wanted to highlight that that is the key part of what we need to accomplish to be successful.

Burkhardt stated that, just to reinforce this, we are looking at a very high level environmental, in the NEPA sense, which means a sort of natural environment and human environment. He said that they will run through a number of those factors, but a lot of them will not differentiate our alternatives at the level that we will get into here, but we do want to sort of run through them quickly using GIS, and once they are available they can provide us some quick information at a high level on affect, is that a potential impact; so a high level environmental screening is what we plan to with the study.

Haugen referred to the Environmental and NEPA/PEL Process Map and stated, just to talk about the termini of the gold lines, on the North Dakota side we have an established street network that is in place, neighborhoods, etc.; and so those touchdown points or termini on the North Dakota side are kind of set in place; on the Minnesota side we don't have that and so what he thinks would be important for us to keep in mind on the Minnesota side is two things, one is that we are going to touch down within the flood protection system, and that could mean being on top of the dike or, as we show here, a little bit further inside the dike but that is point number one and it will be within the dike system, and then point two is that we are going to be considering a three-legged intersection, and where exactly it will be does not necessarily mean that it will be right at the point we show here, however we do know that we don't anticipate doing a lot of work showing how a roadway would extend further east for this purpose we are touching down on; Rhinehart Drive extended into the flood protection system, it is a three-legged intersection and the exact precise location is still in play, but it will be a three-legged intersection based on our anticipated growth and traffic needs.

Mason asked if Mr. Haugen was more or less saying that the eastern limit of the study area is Rhinehart Drive, and where that gold line terminates or will analysis still occur between the larger red box on the map. Haugen responded that it will still occur between the larger red box plus we will also be looking at some of the intersections that are off the screen here, to the east, 220 and 2 probably is the one that is further east that is part of the traffic and intersections that we will look at. Mason stated that that is what he was kind of indicating, and he was curious because the Minnesota Department of Transportation's perspective is how does this interact further to the east and the impacts to the State Highway System. Haugen added that US Business 2 is also going to be heavily impacted by this study as well.

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 19<sup>th</sup>, 2021**

Burkhardt stated that, more or less the conclusion of the study, in terms of evaluating and getting to an understanding of performance and which option might best meet the purpose and need; the point of this is that they will collect a lot of information, again at a screening level, but a lot of different data points in terms of environmental impacts, traffic performance, etc. He said that they will boil those down and focus in on what matters, and he refers to this as the key to their approach, we can talk all day about a certain issue that is maybe an impact that will need to be evaluated in the future, and we need to document that, does it make a difference as to the preference, if it doesn't make a difference between two corridors or two corridors and a no-build, we can acknowledge that and then move on and focus on the things that do matter and that do differentiate in order to understand which one performed better and which one best meets the project objectives.

Burkhardt commented that he has done a lot of studies like this; the evaluation process, the way he sees this is sort of walking a balance between understanding which one performs best to get the criteria that you set, but also not using the process the evaluation as well we plug it into the spreadsheet and whichever one has the most points is the answer; gives you information, tells you how your alternatives, how your option performs, but there is some discussion as to what you do with that information, whether that will be clear and simple or long and difficult we will find out, but that is where we are headed.

Burkhardt stated that that is his formal presentation, a lot in there, and he would definitely love to hear if that raised any questions or concerns, or is there an issue that you didn't hear me talk about and you want to make sure I understand it and hear how we might approach it, he would definitely appreciate hearing from you, you won't hurt his feelings and he would rather hear about it sooner or later.

**QUESTIONS/ANSWERS:**

Sperry said that she has a comment on the public involvement portion; are you going to reach out to more State and Federal Agencies for their input. Burkhardt responded that right now he would say that it is mostly this group, the Technical Advisory Committee, as well as a couple of environmental agencies that we have talked to, is that what you had in mind. Sperry responded yes, especially if you want to tie it into PEL. She added that she isn't sure on the hydraulic study, if anybody had reached out to the Coast Guard to make sure the elevations that were looked at were okay with what they would approve, but then there is also Minnesota DNR and PCA on the Minnesota side that you may want to contact as well. Burkhardt said that he thinks that all makes sense, so maybe we should talk offline unless you have a quick answer to what level of engagements and involvement do you think is appropriate for those agencies, in terms of, he thinks the Coast Guard is great and we do want to understand the context proposed in that hydraulic study are feasible or acceptable from that elevation standpoint. Haugen commented that the perspective he would provide to this, and we have used this in the past, is a simpler solicitation of view to reach out to all of those environmental agencies and try to engage them early and often in the process. He said that we do know, that experience tells us that until we are saying that we actually have a project programmed, their excitement and enjoyment of participating in a planning study is a little bit lower on their totem poles, so if you can help us



**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 19<sup>th</sup>, 2021**

spur their engagement that would be awesome. Sperry responded that she can try to help with that. Burkhardt asked if Ms. Sperry thought that copying them on e-mail is adequate, or maybe sharing or asking for feedback; what level do you think is worthwhile or minimal, in terms of accomplishing the PEL study, how would you like to see that engaged. Sperry responded that she thinks they are in the public involvement portion would be important, to show you documented that you contacted and coordinated with some of those. She said that Minnesota has a lot more regulations than North Dakota and so she would hate to choose something and then have something pop up that wasn't thought of. Burkhardt commented that to Mr. Haugen's credit he has been talking about those agencies all along, and he neglected to mention them.

Kuharenko stated that one of the big issues that they have kind of seen with a bridge study is a lot of the public saying that they didn't have a chance to comment on this; you don't want to have a decision be made or a potential of not having their voice heard so public involvement is a huge huge deal on this bridge discussion. He asked if Mr. Burkhardt could talk a little bit more about public engagement, and particularly direct mailings for residents. Burkhardt responded that this is something that has been discussed briefly but his experience with direct mailings are that they can be really effective, but they are expensive, and they did not include that cost in their budget so he would put Mr. Haugen on the spot as to whether that is something he had thoughts of or that the MPO would do or not do. Haugen responded that we have done them in the past, and he knows the Mr. Burkhardt did send an email regarding it but he has not been able to respond to it as he has not physically been in the office so that is something that we can work on next week. He said though that we do know that there are thousands of properties that are in our study area; we did discuss this at a previous Technical Advisory Committee meeting as well and we did hear them suggest we do a mass mailing, so we are working through the logistics, and we don't have a definitive yes or no at this point. Burkhardt commented that that is a good question; we do hear a lot that people didn't hear about something, and sometimes it is actually true, but in addition to or if we aren't able to do a mailing he thinks the way that we will be getting the word out is the channels that the MPO uses already, which would be social media. He added that the Ad Hoc Group has sort of that responsibility to report back to their constituents so the neighborhood is kept in the loop, so that is a channel we have and again it may not get to every last person, but he thinks we do need to make sure we do everything we can, but in any event he is clear on this issue and he doesn't want us to get tagged on it half way through the study.

Kuharenko said that he has one other question or comment; but he did see that in the alternatives that you are looking at round-a-bouts, traffic signals and those sort of things, one of the things that may come up is, particularly with the nearby schools; if memory serves he thinks the last time we installed a round-a-bout there were some concerns from the school district as to their safety, so that might be a point of discussion you may want to be prepared for. Burkhardt thanked him for the heads up and asked if there were any, or many round-a-bouts in the area. Kuharenko responded that we have a couple; we have one over by Discovery Elementary School, which is located at South 34<sup>th</sup> Street and 40<sup>th</sup> Avenue South, that is probably the closest one we have to a school; and we have another one that is located at 24<sup>th</sup> Avenue South and South 34<sup>th</sup> Street, but it isn't immediately adjacent to a school. He said that the one down by Discovery is probably the most relevant. Burkhardt responded that that helps, and he asked this primarily for familiarity. He added that he understands that for a long time, when you aren't used to them,

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 19<sup>th</sup>, 2021**

you have to get through that to help people understand and get over their concerns, and some people might not like them, but when it comes to safety of pedestrians and bicyclists you do need to be careful where you locate it and how you handle that traffic. He said that that is a good heads up and he doesn't know if we will recommend a round-a- bout at a specific location on the east side, but they just haven't gotten there. He added that Mike Anderson has some experience with that in the area as well, so that is good.

Emery said that he doesn't have any additional comments as Mr. Haugen did address the one question he did have by discussing the touchdown points on the East Grand Forks side, so that was the only question he had as he doesn't think they were even in the hydraulic study, it wasn't ever totally determined where they would be so he is glad that that is still being left open. Burkhardt suggested that we may want to generalize the map and start communicating, but for now it is a nice handy summary that doesn't get too detailed about those kinds of options, but it does show two different touchdown points so maybe those need more of a circled general area around them or some kind of note saying that they aren't the final location options.

Zacher said that he has no questions at this time.

Peterson stated that he really doesn't have anything to add; Mr. Kuharenko's questions and comments on public involvement, which he thinks that will be big, covered most of his concerns as well. He said that some of his previous questions were more of a broad scope of the actual project but more on the planning stage when we talk about the infrastructure supporting such a bridge crossing, so he will save his comments for later in the study.

Mason commented that he saw that Rich Sanders put a comment in, and it may be along the same lines that he was thinking; so if the Future Bridge Traffic Impact Study shows that the touchdown point is more or less just on the inside of the dike, maybe this is part of the implementation plan and further analysis, but what will the network look to the east of there, what will the property impacts and different traffic controls at those intersecting roadways look like. He added that east of the dike the roadway system is basically out of the scope of this project, is that fair enough to say. Haugen responded that we are looking at all of the existing intersections that are in place; we are not anticipating any new roadway beyond connecting to Rhinehart, we will likely have to make improvements to the existing road and intersections that connect Rhinehart to the rest of the network, but we aren't anticipating trying to show that because of the bridge we would be building a new east/west roadway connecting all the way over to Bygland Road or County 72. He added that we do know that right now the jurisdiction of Rhinehart ends at a city limit line, and we do know that in the past, and we heard again from and MPO Board members, that when this bridge is being built the city limits will be used to have jurisdiction of the new road, new bridge, upgrade Rhinehart within the City of East Grand Forks to help ease some of the concerns the Township has expressed already. Haugen commented that they are looking at the existing roadway intersections and the furthest east intersection is just north of the Mallory Bridge, US#2 and 220 South and follow the key intersections all the way back to the landing at Elks and Rhinehart or 32<sup>nd</sup> and Rhinehart.

**PROCEEDINGS OF THE  
TECHNICAL ADVISORY COMMITTEE  
Wednesday, March 19<sup>th</sup>, 2021**

Haugen reported that you do see a Polk County representative on the Ad Hoc Group agency list and we didn't have one from Grand Forks County. He said that the primary reason for this is when they were going through the transportation plan Grand Forks County made it clear that they did not want to weigh in on any City to City bridge location debate discussion, and so because of that input we felt that since this study is looking at just the City to City connection we wouldn't include a Grand Forks County staff as a supporting agency to the Ad Hoc Group, but it certainly doesn't mean that Mr. West isn't able to participate, we just don't anticipate Grand Forks County having a huge involvement in this since they have indicated in the past that they wanted to stay clear.

Sperry asked if they were going to take into consideration in your traffic a new interchange at 47<sup>th</sup> Avenue South in I-29. Haugen responded that that is still up for debate. He added that they will be relying somewhat on the information that we found and discovered during that I-29 Traffic Operation Study that we did, that there is little in our travel demand model making direct traffic connections between this new bridge location and a new interchange location. He explained that the I-29 Study, that was one of the early questions we asked of the Travel Demand Model on the operation of I-29; how or if the potential future additional river crossings impacted I-29, and the simple answer is that this is not a direct correlation between new river crossings and I-29 operations. Burkhardt asked what Ms. Sperry's reaction to that was. Sperry responded that she hasn't read that document so she would defer to Mr. Haugen or anyone else from the group that participated in that. Burkhardt said that he just wants to make sure that we understand that, and he will certainly bring it back to Mike Anderson and their traffic group to make sure we understand that.

Burkhardt stated that he appreciates the input and time from everyone. He said that typically, as you know, we will be doing this as an update as part of the Technical Advisory Committee's regular meetings, and will be focused on what is going on, opportunities for input, and those kinds of exchanges. He added that for him, as you have your ears to the ground that are tuned into issues that may be percolating up or maybe not, he looks forward to further input.

**OTHER BUSINESS**

None.

**ADJOURNMENT**

***MOVED BY BERGMAN, SECONDED BY ELLIS, TO ADJOURN THE MARCH 19<sup>TH</sup>,  
SPECIAL TECHNICAL ADVISORY COMMITTEE MEETING AT 9:46 A.M.***

***MOTION CARRIED UNANIMOUSLY.***

Respectfully submitted by,

Peggy McNelis, Office Manager



**MPO Staff Report**  
**Technical Advisory Committee:**  
April 14, 2021  
**MPO Executive Board:**  
April 21, 2021

**RECOMMENDED ACTION:** Recommend the approval of FY2021 TIP amendments to the MPO Executive Board.

**TAC RECOMMENDED ACTION:** Recommend the approval of FY2021 TIP amendments to the MPO Executive Board subject to any public comment.

Matter of the 2021 TIP Amendment.

**Background:** After the MPO adopts a four year TIP, amendments may need to be process when a project cost estimate changes significantly or the scope of the project changes or federal programs have announced funding awards.

The Washington St Underpass project has had a scope change and a decrease in cost estimate. The scope changed from being a complete reconstruction to a rehabilitation of the underpass structure and reconstruction of the street. The cost decreased form \$17M to \$11M. Of note it that accommodating bike modes is not a part of the rehab portion of the project.

A second amendment adds previously unlisted projects. The first is to add as a funded project the Transportation Alternative to convert the gravel surface multi-use path along 32<sup>nd</sup> Ave S just west of I29 to S. 48<sup>th</sup> St. This project was a candidate project that was not awarded originally. Recent Covid relief acts have provided additional funds; therefore, this project was awarded. A second new project resulted from an inspection of the bike/ped underpass on S. Washington St. The inspection revealed the need to conduct repairs expeditiously. Therefore, a project was funded.

Inspection also has caused the project to do work on the DeMers Overpass structure to be advanced forward one year so that the items noticed during the inspection can be addressed sooner.

A public hearing notice has been published and the proposed amendment available to review prior to the March meetings. The actual hearing will be held during the March TAC meeting, April 14<sup>th</sup>. Comments are able to be submitted until just prior to the meeting; any comments submitted will be announced at the TAC meeting.

**Findings and Analysis:**

- Project changes have been identified.
- The proposed project amendment is consistent with the Metropolitan Transportation Plan.
- A Public Hearing is scheduled for March 10th at the TAC meeting; written comments are

being accepted until 12:00 pm on April 14th.

- These amended projects do add funds so its impact to the TIP remains fiscally constrained.

**Support Materials:**

- Copy of Public Hearing Notice.
- Copy of Proposed Amendments.



## **PUBLIC NOTICE**

The Grand Forks - East Grand Forks Metropolitan Planning Organization (MPO) will hold a public hearing on the proposed amendments to the MPO 2021 to 2024 Transportation Improvement Program (TIP). The TIP also incorporates the local transit operators' Program of Projects (POP). Due to the COVID-19 public health emergency, East Grand Forks City Hall is currently closed to the public. Members of the MPO Technical Advisory Committee will be attending this meeting electronically or telephonically. This meeting will be conducted with social distancing modifications consistent with the recommendations of the CDC. The hearing will be held at 1:30 PM on April 14th. The public, particularly special and private sector transportation providers, are encouraged to provide input via email.

A copy of the proposed amendments is available for review and comment at the MPO website [www.theforksmpo.org](http://www.theforksmpo.org). Written comments on the proposed amendment can be submitted to the email address [info@theforksmpo.org](mailto:info@theforksmpo.org) until noon on April 14th. All comments received prior to noon on the meeting day will be considered part of the record of the meeting as if personally presented. If substantial changes occur to the document due to comments received, the MPO will hold another public hearing on the changes.

For further information, contact Mr. Earl Haugen at 701/746/2660. The GF-EGFMPO will make every reasonable accommodation to provide an accessible meeting facility for all persons. Appropriate provisions for the hearing and visually challenged or persons with limited English Proficiency (LEP) will be made if the meeting conductors are notified 5 days prior to the meeting date, if possible. To request language interpretation, an auxiliary aid or service (i.e., sign language interpreter, accessible parking, or materials in alternative format) contact Earl Haugen of GF-EGFMPO at 701-746-2660. TTY users may use Relay North Dakota 711 or 1-800-366-6888.

Materials can be provided in alternative formats: large print, Braille, cassette tape, or on computer disk for people with disabilities or with LEP by Earl Haugen of GF-EGFMPO at 701-746-2660. TTY users may use Relay North Dakota 711 or 1-800-366-6888.

**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**FISCAL YEARS 2021 - 2024**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS) AND SOURCE OF FUNDING					STAGING	ANNUAL ELEMENT	FUTURE EXPENDITURES				
	RESPONSIBLE AGENCY	CLASSIFICATION								2021	2022	2023	2024		
PROJECT NUMBER	PROJECT TYPE	FUNDING STATUS		TOTAL	FEDERAL	STATE	OTHER	LOCAL	Operations	Capital	P.E.	R.O.W.	CONSTR.	TOTAL	
				FUNDING SOURCE											
Grand Forks #ND17 PCN 22167	Grand Forks	N. Washington	Reconstruct the underpass of the BNSF railway on N. Washington St (US 81B) just north of the intersection with DeMers Ave (ND297)	REMARKS: STIP shows as two separate projects Aproximately 50% funding through Regional Urban and other 50% funding through Rural Program Amended April 2021											
	NDDOT	Principle Arterial								Operations					
	Bridge Reconstruct	Discretionery	Reconstruct the roadway, rehabilitate the structure and make sidewalks ADA compliant for the railroad underpass on US 81B (N. Washington St) just north of the intersection of ND 297 (DeMers Ave).	11,150.00	9,024.00	1,063.00		1,063.00	P.E.						
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.						
			47,600.00	44,244.00	4,596.00		4,760.00	CONSTR.		11,150.00					
			Urban Regional Secondary Roads Program					TOTAL			11,150.00				
Grand Forks #ND18 No PCN	Grand Forks	varies	The City of Grand Forks will rehab traffic signals on the Urban Road system throughout Grand forks	REMARKS:											
	Grand Forks	varies							Operations						
	ITS Rehab	Discretionery							Capital						
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	P.E.						
			3,100.00	2,280.00	0.00	0.00	820.00	R.O.W.							
			Urban Roads Program					CONSTR.		3,100.00					
								TOTAL		3,100.00					
Grand Forks #ND19 PCN 22871	Grand Forks	N. 4th St	reconstruction of N. 4th St between DeMers Ave and 1st Ave N including streetscaping components	REMARKS: Governor's Main Street Initiative											
	Grand Forks	Minor Arterial							Operations						
	Reconstruction	Discretionery							Capital						
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	P.E.						
			2,305.00	1,631.00			673.80	R.O.W.							
			Urban Grant Program					CONSTR.		2,305.00					
								TOTAL							

**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**FISCAL YEARS 2021 - 2024**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS) AND SOURCE OF FUNDING					STAGING	ANNUAL ELEMENT	FUTURE EXPENDITURES					
	RESPONSIBLE AGENCY	CLASSIFICATION								2021	2022	2023	2024			
PROJECT NUMBER	PROJECT TYPE	FUNDING STATUS		TOTAL	FEDERAL	STATE	OTHER	LOCAL	Operations	Capital	P.E.	R.O.W.	CONSTR.	TOTAL		
				FUNDING SOURCE												
Grand Forks #ND20 PCN 22909	Grand Forks	US Bus2	complete a chip seal on US Bus2 (N. 5th St) between DeMe and Gateway Dr	REMARKS:												
	NDDOT	Minor Arterial														
	Rehabilitation	Discretionary														
				100.00	81.00	9.00	0.00	10.00					100.00			
				Urban Regional Secondary Roads Program					TOTAL					100.00		
Grand Forks #ND21 No PCN	Grand Forks	32nd Ave S	convert a gravel surfaced multi-use trail into a hard surfaced multi-use trail between S. 48th St and Heartland Dr.	REMARKS: Recent award of funds due to increase funding from COVID relief funding Amended April 2021												
	Grand Forks	Minor Arterial												90.00		
	New Construction	Discretionary												302.00		
				392.00	236.00			156.00						392.00		
				Urban Regional Secondary Roads Program					TOTAL					392.00		
Grand Forks #ND22 PCN 23192	Grand Forks		perform maintenance work on the bike/ped underpass structure on S. Washington St. (US 81B) near 24th Ave S.	REMARKS: Inspection of structure identified need to have this project New project Amended April 2021												
	NDDOT															
	Preventative Maint.													50.00		
				50.00	40.50	4.50		5.00						50.00		
				Urban Regional Secondary Roads Program					TOTAL					50.00		



**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**FISCAL YEARS 2021 - 2024**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS) AND SOURCE OF FUNDING					STAGING	ANNUAL ELEMENT				
										FUTURE EXPENDITURES				2021
	PROJECT NUMBER	RESPONSIBLE AGENCY		CLASSIFICATION	TOTAL	FEDERAL	STATE	OTHER	LOCAL	Operations				
		PROJECT TYPE		FUNDING STATUS						R.O.W.	CONSTR.			
Grand Forks #ND25 PCN	Grand Forks	I29	CPR, grinding of I29 near the 32nd Ave S Interchange and southward to Thompson Interchange. Both directions	REMARKS: STIP has listed as two separate projects 3 miles is within MPO Study area										
	NDDOT	Interstate							Capital					
	Rehabilitation	Discretionary							P.E.					
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.	CONSTR.				
				1,982.00	1,784.00	198.00	0.00	0.00				1,982.00		
				Interstate Maintenance Program					TOTAL			1,982.00		
Grand Forks #ND26 PCN	Grand Forks	DeMers Overpass	Structural rehabilitation of the DeMers (ND297) Overpass of BNSF and 4th Ave S	REMARKS: Listed in the STIP as 4th Ave S (BNRR Overpass) 297-2.696 inspection caused project to move up one year AMENDED April 2021										
	NDDOT	Principal Arterial							Capital					
	Rehabilitation	Discretionary							P.E.					
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.	CONSTR.				
				834.00	675.35	75.69		83.45				750.00	834.00	
				Bridge Program					TOTAL		750.00	834.00		
Grand Forks #ND27 PCN	Grand Forks	I29	High Tension Median Cable Guardrail Fargo District to Grand Forks	REMARKS: portion inside the MPO Planning Area										
	NDDOT	Interstate							Capital					
	Safety	Discretionary							P.E.					
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.	CONSTR.				
				4,100.00	3,690.00	410.00						4,100.00		
				Highway Safety Improvement Program					TOTAL			4,100.00		

**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**Illustrative Projects**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS)					STAGING	Pending Year
	RESPONSIBLE AGENCY	CLASSIFICATION		AND SOURCE OF FUNDING					Operations	2022
PROJECT NUMBER	PROJECT TYPE	FUNDING STATUS		TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.	
				FUNDING SOURCE					CONSTR.	
				TOTAL						
Grand Forks #ND1 III	Grand Forks	Maintenance Bldg	Expansion of the Public Transportation Maintenance Building and new fueling system	REMARKS: Project is applying for competitive grant programs						
	Grand Forks	Capital		Operations						
No PCN	Facility Expansion	Discretionary							Capital	6,000.00
									P.E.	
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.	
				6,000.00	4,800.00			1,200.00	CONSTR.	
				FTA Programs					TOTAL	6,000.00
				REMARKS:						
									Operations	
									Capital	
									P.E.	
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.	
									CONSTR.	
				TOTAL						
				REMARKS:						
									Operations	
									Capital	
									P.E.	
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.	
									CONSTR.	
				TOTAL						



# **MPO Staff Report**

**Technical Advisory Committee: April 14, 2020**  
**MPO Executive Board:**  
**April 21, 2020**

**RECOMMENDED ACTION: Recommend the approval of draft FY2022-2025 MN Side TIP to the MPO Executive Board,**

**TAC RECOMMENDED ACTION: Recommend the approval of draft FY2022-2025 MN Side TIP to the MPO Executive Board,**

Matter of the Draft FY2022-2025 MN side TIP.

**Background:** Annually, the MPO, working in cooperation with the state dots and transit operators, develop a Transportation Improvement Program (TIP), which also serves as the transit operators' Program of Projects (POP). The TIP covers a four period and identifies all transportation projects scheduled to have federal transportation funding during the four year period. The process runs over an eleven month period with several public meetings ranging from solicitation of projects for specific programs and comments on listed projects. This point in the process is the documenting of the draft TIP.

The Minnesota side of the draft TIP has been cooperatively developed. The North Dakota side is still pending this cooperative process. The public hearing is scheduled for April 14<sup>th</sup> TAC meeting. Written comments are due by noon April 14<sup>th</sup>.

A few changes to point out. The transit capital purchases that are in the current TIP are being modified in this draft. Another project of note is the replacement of the traffic signals on DeMers Ave in the downtown area of East Grand Forks. The draft TIP now has the cost at \$1.3M with federal participation remaining the same yet the match changing to more being paid for by the City. A new project is listed for making improvements (yet to be fully determined) for the railroad crossing at 2<sup>nd</sup> Ave NE, which is estimated at \$300,000.

The TAC and MPO Executive Board will be requested to adopt the MN side draft TIP for 2022-2025.

### **Findings and Analysis:**

- The projects listed are consistent with the MPO's Metropolitan Transportation Plan.
- The projects listed are consistent with the draft MN draft ATIP, still being finalized.
- The projects have identified funding and therefore the TIP is fiscally constrained.

### **Support Materials:**

- Copy of draft 2022-2025 MN side TIP Submitted to Public Comment
- Copy of Public Notice



## PUBLIC NOTICE

The Grand Forks - East Grand Forks Metropolitan Planning Organization (MPO) will hold a public hearing on the Minnesota Side Draft MPO 2022 to 2025 Transportation Improvement Program (TIP). The TIP also incorporates the local transit operators' Program of Projects (POP). Due to the COVID-19 public health emergency, East Grand Forks City Hall is currently closed to the public. Members of the MPO Technical Advisory Committee will be attending this meeting electronically or telephonically. This meeting will be conducted with social distancing modifications consistent with the recommendations of the CDC. The hearing will start at 1:30 PM on April 14<sup>th</sup>. The public, particularly special and private sector transportation providers, are encouraged to consider providing input.

The draft TIP lists all transportation improvement projects programmed to be completed between the years of 2022 to 2025 on the Minnesota side of the Red River. A separate draft for the North Dakota side will be done later and notice will be given when it is ready. A copy of the draft TIP is available for review and comment at the MPO website [www.theforksmpo.org](http://www.theforksmpo.org). Written comments on the draft TIP can be submitted to the email address [info@theforksmpo.org](mailto:info@theforksmpo.org) until noon on April 14<sup>th</sup>. All comments received prior to noon on the meeting day will be considered part of the record of the meeting as if personally presented.

For further information, contact Mr. Earl Haugen at 701/746/2660. The GF-EGFMPO will make every reasonable accommodation to provide an accessible meeting facility for all persons. Appropriate provisions for the hearing and visually challenged or persons with limited English Proficiency (LEP) will be made if the meeting conductors are notified 5 days prior to the meeting date, if possible. To request language interpretation, an auxiliary aid or service (i.e., sign language interpreter, accessible parking, or materials in alternative format) contact Earl Haugen of GF-EGFMPO at 701-746-2660. TTY users may use Relay North Dakota 711 or 1-800-366-6888.

Materials can be provided in alternative formats: large print, Braille, cassette tape, or on computer disk for people with disabilities or with LEP by Earl Haugen of GF-EGFMPO at 701-746-2660. TTY users may use Relay North Dakota 711 or 1-800-366-6888.



Grand Forks - East Grand Forks

METROPOLITAN  
PLANNING ORGANIZATION

**MINNESOTA SIDE  
Draft  
TRANSPORTATION  
IMPROVEMENT  
PROGRAM**

**2022 - 2025**

**April, 2021**

FISCAL YEARS 2022 - 2025  
MINNESOTA SIDE Draft  
TRANSPORTATION IMPROVEMENT PROGRAM

FOR THE  
GRAND FORKS - EAST GRAND FORKS  
METROPOLITAN AREA

PREPARED BY:  
THE GRAND FORKS - EAST GRAND FORKS  
METROPOLITAN PLANNING ORGANIZATION

**METROPOLITAN PLANNING ORGANIZATION**  
**MEMBERSHIP**

**Bob Rost**

REPRESENTING:  
GRAND FORKS COUNTY  
COMMISSION

**Warren Strandell**

REPRESENTING:  
POLK COUNTY COMMISSION

**Ken Vien**

REPRESENTING:  
GRAND FORKS CITY  
COUNCIL

**Clarence Vetter**

REPRESENTING:  
EAST GRAND FORKS CITY  
COUNCIL

**Jeannie Mock**

REPRESENTING:  
GRAND FORKS CITY  
COUNCIL

**Marc DeMers**

REPRESENTING:  
EAST GRAND FORKS CITY  
COUNCIL

**Al Grasser**

REPRESENTING:  
GRAND FORKS PLANNING  
& ZONING COMMISSION

**Michael Powers**

REPRESENTING:  
EAST GRAND FORKS PLANNING  
& ZONING COMMISSION

**TRANSPORTATION PLANNING PROCESS  
CERTIFICATION STATEMENT**

The Grand Forks – East Grand Forks Metropolitan Planning Organization for the Grand Forks, North Dakota, and East Grand Forks, Minnesota, metropolitan region hereby certifies that it is carrying out a continuing, cooperative and comprehensive transportation planning process for the region in accordance with the applicable requirements of:

- 23 USC 134 and 49 USC 5303, and 23 CFR Part 450;
- In non-attainment and maintenance areas, sections 174 and 176 (c) and (d) of the Clean Air Act, as amended (42 U.S.C. 7504, 7506 (c) and (d)) and 40 CFR part 93;
- Title VI of the Civil Rights Act of 1964, as amended (42 U.S.C. 2000d-1) and 49 CFR part 21;
- 49 U.S.C. 5332, prohibiting discrimination on the basis of race, color, creed, national origin, sex, or age in employment or business opportunity;
- Section 1101(b) of the Fixing America’s Surface Transportation (FAST) (Pub. L. 114-357) and 49 CFR part 26 regarding the involvement of Disadvantaged Business Enterprises in USDOT funded planning projects;
- 23 CFR part 230, regarding the implementation of an equal employment opportunity program on Federal and Federal-aid highway construction contracts;
- The provisions of the Americans with Disabilities Act of 1990 (42 U.S.C. 12101 et seq.) and 49 CFR parts 27, 37, and 38;
- The Older Americans Act, as amended (42 U.S.C. 6101), prohibiting discrimination on the basis of age in programs or activities receiving Federal financial assistance;
- Section 324 of Title 23 U.S.C. regarding the prohibition of discrimination based on gender; and
- Section 504 of the Rehabilitation Act of 1973 (29 U.S.C. 794) and 49 CFR part 27 regarding discrimination against individuals with disabilities.

GF-EGF MPO  
Metropolitan Planning  
Organization

Minnesota Department  
of Transportation

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Chair

\_\_\_\_\_  
Director

\_\_\_\_\_  
Date

\_\_\_\_\_  
Date



**A RESOLUTION APPROVING FY 2022 - FY 2025  
MINNESOTA SIDE  
DRAFT TRANSPORTATION IMPROVEMENT PROGRAM FOR THE  
GRAND FORKS-EAST GRAND FORKS METROPOLITAN AREA**

**WHEREAS**, the U.S. Department of Transportation requires the development and annual updating of a draft Transportation Improvement Program (TIP) for each urbanized area under the direction of a Metropolitan Planning Organization; and

**WHEREAS**, projects must be included in the draft TIP in accordance with 23 CFR 450.324 (f) (1); and

**WHEREAS**, local transit projects utilizing Federal Transit Administration Section 5307 funds must be listed in a Program of Projects (49 U.S.C. 5307 c); and

**WHEREAS**, local projects of regional significance without federal funding are included, and

**WHEREAS**, the Grand Forks-East Grand Forks Metropolitan Planning Organization has been designated as the urban policy body with responsibility for performing urban transportation planning and required reviews; and

**WHEREAS**, the Grand Forks-East Grand Forks Metropolitan Planning Organization is designated by the Governors of North Dakota and Minnesota as the body responsible for making transportation planning decisions in the Grand Forks-East Grand Forks Metropolitan Area; and

**WHEREAS**, Presidential Executive Order 12372 gave state government the flexibility to design their own review process and select federal programs and activities to be subject to the process. Wherein, North Dakota Executive Order 1984-1 establishes the North Dakota Federal Program Review process and exempts the Transportation Improvement Program (TIP) from said process; and

**WHEREAS**, the projects contained in the TIP are located in an area where both the North Dakota and Minnesota State Implementation plans for Air Quality are not required to contain any transportation control measures. Therefore, the conformity procedures do not apply to these projects; and

**WHEREAS**, projects contained in the draft Minnesota Side T.I.P. were developed in cooperation with the Minnesota Department of Transportation, the local public transit operator and the MPO; and

**WHEREAS**, the Technical Advisory Committee has recommended approval of the draft Minnesota Side TIP after having held a public hearing on the Draft TIP on April 14, 2021.

**NOW, THEREFORE, BE IT RESOLVED**, that the Grand Forks-East Grand Forks Metropolitan Planning Organization approves the Grand Forks-East Grand Forks Metropolitan Area Draft Minnesota Side Transportation Improvement Program for the FY 2022 to FY 2025 program period as being consistent with the Metropolitan Transportation Plan and the area's plans and program included therein.

\_\_\_\_\_  
Date

\_\_\_\_\_  
Jeannie Mock, Chairman

\_\_\_\_\_  
Date

\_\_\_\_\_  
Earl Haugen, Executive Director

**A RESOLUTION CONFIRMING THE  
METROPOLITAN TRANSPORTATION PLAN  
AS BEING CURRENTLY HELD VALID**

**WHEREAS**, the **23 U.S.C. 134** requires that the Metropolitan Planning Organization (MPO) designated with the authority to carry out metropolitan transportation planning in a given urbanized area shall prepare a transportation plan for that area; and

**WHEREAS**, the Grand Forks-East Grand Forks Metropolitan Planning Organization has been designated by the Governors of the States of Minnesota and North Dakota as the MPO for the Grand Forks-East Grand Forks Metropolitan Area; and

**WHEREAS**, the Grand Forks - East Grand Forks MPO has a Transportation Plan composed of a Metropolitan Transportation Plan (adopted January 23, 2019); and

**WHEREAS**, the Technical Advisory Committee of the Grand Forks - East Grand Forks MPO has recommended that this Metropolitan Transportation Plan be considered currently held valid and consistent with current transportation and land use considerations.

**NOW, THEREFORE, BE IT RESOLVED THAT**, the Grand Forks-East Grand Forks Metropolitan Planning Organization certifies that the Metropolitan Transportation Plan for the Grand Forks-East Grand Forks Urbanized Area is currently held valid and consistent with current transportation and land use considerations.

\_\_\_\_\_  
Date

\_\_\_\_\_  
Jeannie Mock ,  
Chairman

\_\_\_\_\_  
Earl T. Haugen,  
Executive Director

## **TABLE OF CONTENTS**

	<b><u>PAGE</u></b>
TRANSPORTATION IMPROVEMENT PROGRAM .....	1
INTRODUCTION .....	1
MINNESOTA AREA PROJECT LISTINGS .....	2

## **TABLE OF APPENDICES**

	<b><u>PAGE</u></b>
APPENDIX I NWATP DRAFT ATIP .....	19
APPENDIX II MPO STUDY AREA MAP .....	21

## **LIST OF MAPS**

	<b><u>PAGE</u></b>
MAP 1: GRAND FORKS/EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION STUDY AREA .....	22

## INTRODUCTION

The draft Transportation Improvement Program (TIP) for the Grand Forks -East Grand Forks area lists the significant transportation system improvements to be implemented during the next four years. The draft 2022-2025 TIP is submitted under the Fixing America's Surface Transportation (FAST). This Act was adopted in 2014.

Federal requirements stipulate each state must develop a TIP, and project selection must be performed in cooperation with the MPOs. Similarly, local TIP's must be developed in cooperation with the State. The TIP is updated annually, and encompasses a 4-year time period. Projects may be programmed for periods beyond 4 years, provided they are prioritized, and financial funding sources dedicated to transportation uses are identified. In order to remain consistent with these requirements, projects programmed for 2022 are considered the Annual Element, and Program Years 2023, 2024 and 2025 are designated as Future Year projects.

The projects which comprise the draft TIP were developed, studied, and evaluated as part of the Metropolitan "3C" Transportation Planning Processes, which has been established in the Grand Forks - East Grand Forks Area. The TIP may be modified at any time, consistent with procedures established for its development, and consistent with the Transportation Plan. Each year the TIP process is unique. However, there are some common "significant differences" during the development of each TIP. The addition of a project, or expansion of its scope, not on the advance review material would constitute a difference that would require additional public input before final adoption. If a project's local share is increased by over 25% the amount identified in advance, the difference would require additional public input. A decrease, on the other hand, would not. Changing the source of state or federal funds would constitute a significant difference. The modification criteria are identified in the MPO's Public Participation Plan.

The Federal Highway Administration (FHWA), and the Federal Transportation Administration (FTA) require that in order for certain projects to be funded with federal assistance, those projects must be included in a Transportation Improvement Program (TIP) approved by the appropriate Metropolitan Planning Organization (MPO). In the Grand Forks-East Grand Forks Metropolitan Area, the Grand Forks-East Grand Forks Metropolitan Planning Organization is the designated MPO. FHWA and FTA require federally funded projects located within the boundaries of the "Study Area" (see map in Appendix II), and funded from any of the categories of federal aid to be in a MPO approved TIP.

The MPO staff worked with the local communities and State Departments of Transportation to prepare the draft FY 2022-2025 Transportation Improvement Program for the Grand Forks-East Grand Forks Metropolitan Area. The MPO utilizes the 2045 Metropolitan Transportation Plan to ensure projects are consistent with the MTP's priorities.

**MINNESOTA AREA  
PROJECT LISTINGS**

**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**FISCAL YEARS 2022 - 2025**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS) AND SOURCE OF FUNDING					STAGING	ANNUAL ELEMENT	FUTURE EXPENDITURES			
	RESPONSIBLE AGENCY	CLASSIFICATION								2022	2023	2024	2025	
PROJECT NUMBER	PROJECT TYPE	FUNDING STATUS		TOTAL	FEDERAL	STATE	OTHER	LOCAL	Operations					
				FUNDING SOURCE					CONSTR.					
East Grand Forks #MN1	East Grand Forks	NA	Operating subsidy for proposed East Grand Forks fixed-route transit service. The service will operate 6 days a week and averages 36 hours of revenue service daily. Bus for the period January 1, 2022 to December 31, 2022 (Costs for fixed-route service are estimates).  TRF-0018-22B	REMARKS: Contract fixed route services with City of Grand Forks Estimated payment to GF is \$530,000 <b>The Federal and Local revenues may be replaced by CARES</b> Estimated fare is \$10,000 <b>Other is MN Transit Formula Funds</b>										
	East Grand Forks	Operations							Operations	606.20				
	Fixed-Route Transit Service	Entitlement								Capital	0.00			
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	P.E.	NA				
				606.20	135.00	0.00	349.80	121.40	R.O.W.	NA				
				FTA 5307					CONSTR.	NA				
									TOTAL	606.20				
East Grand Forks #MN2	East Grand Forks	NA	Operating subsidy for demand response service for disabled persons and senior citizens covering the period January 1, 2022 to December 31, 2022. The paratransit service operates the same hours of operation as the fixed-route transit service (costs for paratransit service are estimates).  TRF-0018-22A	REMARKS: Contract demand response service Estimated fare is \$18,000 <b>The Local revenues may be replaced by CARES</b> <b>Other is MN Transit Formula Funds</b>										
	East Grand Forks	Operations							Operations	112.50				
	Paratransit Service for Disabled Persons	Entitlement							Capital	0.00				
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	P.E.	NA				
				112.50	0.00	0.00	95.63	16.87	R.O.W.	NA				
				State Transit Funds					CONSTR.	NA				
									TOTAL	112.50				
East Grand Forks #MN3	East Grand Forks	NA	Purchase Class 500 replacement vehicle for Demand Response  TRS-0018-22T	REMARKS:										
	East Grand Forks	Capital							Operations	0.00				
	Paratransit Service for Disabled Persons	Entitlement							Capital	98.00				
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	P.E.	NA				
				98.00	78.50			19.50	R.O.W.	NA				
									CONSTR.	NA				
									TOTAL	98.00				

**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**FISCAL YEARS 2022-2025**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS) AND SOURCE OF FUNDING					STAGING	ANNUAL ELEMENT	FUTURE EXPENDITURES			
	RESPONSIBLE AGENCY	CLASSIFICATION								2022	2023	2024	2025	
PROJECT NUMBER	PROJECT TYPE	FUNDING STATUS		TOTAL	FEDERAL	STATE	OTHER	LOCAL	Operations					
									Capital					
									P.E.					
									R.O.W.					
									CONSTR.					
FUNDING SOURCE									TOTAL					
East Grand Forks #MN4	East Grand Forks	US 2	WBL - FROM 5TH AVE NW (EAST GRAND FORKS) TO 0.3 MI E OF POLK CSAH 15 (FISHER), RESURFACING	REMARKS: Likely can include alternative concepts currently being considered in US 2 Study.										
	MnDOT	Principal Arterial												
	Rehabilitation	Discretionary							Project # 6001-61	TOTAL	FEDERAL	STATE	OTHER	LOCAL
	District Managed Program									CONSTR.	10,200.00			
									TOTAL	10,200.00				
East Grand Forks #MN5	East Grand Forks	Bygland Rd	reconstruct the intersection of Bygland Road and Rhinehart Drive into a roundabout	REMARKS: Other costs are non-construction costs Other Revenue is MN State Aid										
	East Grand Forks	Minor Arterial												
	Reconstruction	Discretionary							Project # 119-119-013	TOTAL	FEDERAL	STATE	OTHER	LOCAL
	NWATP City Sub-target									CONSTR.	1,493.00			
									TOTAL	1,867.00				
East Grand Forks #MN6	East Grand Forks	Mn220 N	Project entails refurbishing traffic signals at intersection with 14th St NW, make ped improvements at intersection of US 2 and at 17th St NW; includes signal enhancements. at interswecion with US2	REMARKS:										
	MnDOT	Minor Arterial												
	Rehabilitation	Discretionary							Project #6017-44	TOTAL	FEDERAL	STATE	OTHER	LOCAL
	District Managed Program									CONSTR.	410.00			
									TOTAL	410.00				



**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**FISCAL YEARS 2022 - 2025**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS) AND SOURCE OF FUNDING					STAGING	ANNUAL ELEMENT	FUTURE EXPENDITURES							
	RESPONSIBLE AGENCY	CLASSIFICATION								2022	2023	2024	2025					
PROJECT NUMBER	PROJECT TYPE	FUNDING STATUS		TOTAL	FEDERAL	STATE	OTHER	LOCAL	Operations									
				FUNDING SOURCE					CONSTR.									
									TOTAL									
East Grand Forks #MN7	East Grand Forks	NA	Operating subsidy for proposed East Grand Forks fixed-route transit service. The service will operate 6 days a week and averages 36 hours of revenue service daily. Bus for the period January 1, 2023 to December 31, 2023 (Costs for fixed-route service are estimates).  TRF-0018-23B	REMARKS: Contract fixed route services with City of Grand Forks Estimated payment to GF is \$545,000  Estimated fare is \$10,000 <b>Other is MN Transit Formula Funds</b>														
	East Grand Forks	Operations							Operations		620.33							
	Fixed-Route Transit Service	Entitlement								Capital		0.00						
									P.E.		NA							
									R.O.W.		NA							
									CONSTR.		NA							
									TOTAL		620.33							
									FTA 5307					TOTAL		620.33		
East Grand Forks #MN8	East Grand Forks	NA	Operating subsidy for demand response service for disabled persons and senior citizens covering the period January 1, 2023 to December 31, 2023. The paratransit service operates the same hours of operation as the fixed-route transit service (costs for paratransit service are estimates).  TRF-0018-23A	REMARKS: Contract demand response service Estimated fare is \$18,000  <b>Other is MN Transit Formula Funds</b>														
	East Grand Forks	Operations							Operations		115.88							
	Paratransit Service for Disabled Persons	Entitlement								Capital		0.00						
									P.E.		NA							
									R.O.W.		NA							
									CONSTR.		NA							
									TOTAL		115.88							
									State Transit Funds					TOTAL		115.88		
East Grand Forks #MN9	East Grand Forks	NA	Purchase Class 500 replacement vehicle for Demand Response  TRF-0018-23T	REMARKS:														
	East Grand Forks	Capital							Operations		0.00							
	Paratransit Service for Disabled Persons	Entitlement								Capital		160.00						
									P.E.		NA							
									R.O.W.		NA							
									CONSTR.		NA							
									TOTAL		160.00							
									Flexed STPBC Program					TOTAL		160.00		

**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**FISCAL YEARS 2022 - 2025**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS) AND SOURCE OF FUNDING					STAGING	ANNUAL ELEMENT	FUTURE EXPENDITURES					
	RESPONSIBLE AGENCY	CLASSIFICATION								2022	2023	2024	2025			
PROJECT NUMBER	PROJECT TYPE	FUNDING STATUS		TOTAL	FEDERAL	STATE	OTHER	LOCAL	Operations	Capital	P.E.	R.O.W.	CONSTR.	TOTAL		
				FUNDING SOURCE												
East Grand Forks #MN10	MnDOT	2nd Ave NE	BNSF RR, REPLACE EXISTING SIGNAL SYSTEM AT MSAS 119, 2ND AVE NE, EAST GRAND FORKS, POLK COUNTY	REMARKS:												
	East Grand Forks	Minor Arterial		Other is MN Office of Freight Funds							0.00					
	RR xing	Discretionary		TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.	CONSTR.	P.E.	NA	NA	300.00		
				300.00	270.00	0.00	30.00	0.00						300.00		
				TOTAL					TOTAL							
East Grand Forks #MN11			Intentionally left blank	REMARKS:												
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.	CONSTR.	P.E.					
				TOTAL					TOTAL							
East Grand Forks #MN12			Intentionally left blank	REMARKS:												
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.	CONSTR.	P.E.					
				TOTAL					TOTAL							

**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**FISCAL YEARS 2022 - 2025**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS) AND SOURCE OF FUNDING					STAGING	ANNUAL ELEMENT	FUTURE EXPENDITURES							
	RESPONSIBLE AGENCY	CLASSIFICATION								2022	2023	2024	2025					
PROJECT NUMBER	PROJECT TYPE	FUNDING STATUS		TOTAL	FEDERAL	STATE	OTHER	LOCAL	Operations									
				FUNDING SOURCE					CONSTR.									
										TOTAL								
East Grand Forks #MN13	East Grand Forks	NA	Operating subsidy for proposed East Grand Forks fixed-route transit service. The service will operate 6 days a week and averages 36 hours of revenue service daily. Bus for the period January 1, 2024 to December 31, 2024 (Costs for fixed-route service are estimates).	REMARKS: Contract fixed route services with City of Grand Forks Estimated payment to GF is \$560,000														
	East Grand Forks	Operations		Estimated fare is \$10,000 <b>Other is MN Transit Formula Funds</b>								639.90						
	Fixed-Route Transit Service	Entitlement	TRF-0018-24B	TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.			NA						
				639.90	140.00	0.00	371.10	128.80	CONSTR.			NA						
										FTA 5307				TOTAL			639.90	
East Grand Forks #MN14	East Grand Forks	NA	Operating subsidy for demand response service for disabled persons and senior citizens covering the period January 1, 2024 to December 31, 2024. The paratransit service operates the same hours of operation as the fixed-route transit service (costs for paratransit service are estimates)	REMARKS: Contract demand response service Estimated fare is \$18,000														
	East Grand Forks	Operations		<b>Other is MN Transit Formula Funds</b>								119.36						
	Paratransit Service for Disabled Persons	Entitlement	TRF-0018-24A	TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.			NA						
				119.36	0.00	0.00	101.46	17.90	CONSTR.			NA						
										State Transit Funds				TOTAL			119.36	
East Grand Forks #MN15	East Grand Forks	NA	<del>Purchase Class 500 replacement vehicle</del> Purchase Class 400 replacement vehicle	REMARKS:														
	East Grand Forks	Capital		<b>Other is MN Transit Formula Funds</b>								0.00						
	Fixed-Route Transit Service	Entitlement	TRF-0018-24C	104.00	83.20			20.80	P.E.			NA						
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.			NA						
				180.00	144.00	0.00	0.00	36.00	CONSTR.			NA						
										FTA #5307 Flexed STPBG Program FHWA				TOTAL			104.00	

**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**FISCAL YEARS 2022 - 2025**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS) AND SOURCE OF FUNDING					STAGING	ANNUAL ELEMENT	FUTURE EXPENDITURES			
	RESPONSIBLE AGENCY	CLASSIFICATION								2022	2023	2024	2024	
PROJECT NUMBER	PROJECT TYPE	FUNDING STATUS		TOTAL	FEDERAL	STATE	OTHER	LOCAL	Operations					
				FUNDING SOURCE					CONSTR.					
									TOTAL					
East Grand Forks #MN16	East Grand Forks	DeMers Ave	On DeMers Ave (USB2), AT 2ND ST NW & 4TH ST NW, SIGNAL SYSTEM REPLACEMENT/ADA IMPROVEMENTS	REMARKS:										
	MnDOT	Principal Arterial	Project # 6001-68	1,300.00	653.60	163.40		483.00				0.00		
	Signal Replacement	Discretionary		TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.			NA		
				4,200.00	680.00	170.00	0.00	350.00	CONSTR.			1,300.00		
				Statewide Performance Program					TOTAL				1,300.00	
East Grand Forks #MN17			Intentionally left blank	REMARKS:										
									Operations			0.00		
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.			NA		
									CONSTR.			NA		
									TOTAL					
East Grand Forks #MN18			Intentionally left blank	REMARKS:										
									Operations			0.00		
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.			NA		
									CONSTR.			NA		
									TOTAL					

**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**FISCAL YEARS 2022 - 2025**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS) AND SOURCE OF FUNDING					STAGING	ANNUAL ELEMENT	FUTURE EXPENDITURES								
	RESPONSIBLE AGENCY	CLASSIFICATION								2022	2023	2024	2025						
PROJECT NUMBER	PROJECT TYPE	FUNDING STATUS		TOTAL	FEDERAL	STATE	OTHER	LOCAL	Operations										
				FUNDING SOURCE					CONSTR.										
										TOTAL									
East Grand Forks #MN19	East Grand Forks	NA	Operating subsidy for proposed East Grand Forks fixed-route transit service. The service will operate 6 days a week and averages 36 hours of revenue service daily. Bus for the period January 1, 2025 to December 31, 2024 (Costs for fixed-route service are estimates).	REMARKS: Contract fixed route services with City of Grand Forks Estimated payment to GF is \$560,000															
	East Grand Forks	Operations		Estimated fare is \$10,000 <b>Other is MN Transit Formula Funds</b>										655.90					
	Fixed-Route Transit Service	Entitlement	TRF-0018-25B	TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.				NA						
				655.90	143.50	0.00	380.38	132.02	CONSTR.				NA						
										FTA 5307					TOTAL				655.90
East Grand Forks #MN20	East Grand Forks	NA	Operating subsidy for demand response service for disabled persons and senior citizens covering the period January 1, 2025 to December 31, 2025. The paratransit service operates the same hours of operation as the fixed-route transit service (costs for paratransit service are estimates)	REMARKS: Contract demand response service Estimated fare is \$18,000															
	East Grand Forks	Operations		<b>Other is MN Transit Formula Funds</b>										122.34					
	Paratransit Service for Disabled Persons	Entitlement	TRF-0018-25A	TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.				NA						
				122.34	0.00	0.00	104.00	18.35	CONSTR.				NA						
										State Transit Funds					TOTAL				122.34
East Grand Forks #MN21			Intentionally left blank	REMARKS:															
				<b>Other is MN Transit Formula Funds</b>										0.00					
				TOTAL	FEDERAL	STATE	OTHER	LOCAL	R.O.W.				NA						
									CONSTR.				NA						
										TOTAL									

**GRAND FORKS - EAST GRAND FORKS METROPOLITAN PLANNING ORGANIZATION**

**TRANSPORTATION IMPROVEMENT PROGRAM**

**TRANSPORTATION IMPROVEMENT PROGRAM**

URBAN AREA	PROJECT LOCATION	FACILITY	PROJECT DESCRIPTION	ESTIMATED COST (THOUSANDS) AND SOURCE OF FUNDING					STAGING	ANNUAL	FUTURE			
	RESPONSIBLE AGENCY	CLASSIFICATION								ELEMENT	EXPENDITURES			
PROJECT NUMBER	PROJECT TYPE	FUNDING STATUS	TOTAL	FEDERAL	STATE	OTHER	LOCAL	Operations	2021	2022	2023	2024		
									Capital	P.E.	R.O.W.	CONSTR.		
FUNDING SOURCE								TOTAL						
			East Grand Forks TOTALS						Other	162.00				
									Operations	718.70	736.21	759.26	778.24	
									Capital	98.00	0.00	104.00	0.00	
									P.E.	150.00	0.00	NA	NA	
									R.O.W.	62.00	0.00	NA	NA	
				17,271.41	10,658.80	2,493.40	2,738.15	1,381.06	CONSTR.	12,103.00	300.00	1,300.00		
									TOTAL	13,293.70	1,036.21	2,163.26	778.24	

# **APPENDIX I**

## **DRAFT NWATP ATIP**

Rte_Sys	Projnum	#Year	Who	Agency	MPO	Description (TECHNICAL DESCRIPTION)	Length	City	County Name	Program	Proposed Funds	STIP Total	Total FHWA	FTA	Total TH	Other
TRANSIT	TRF-0018-22A	2022	L	EAST GRAND FORKS	Grand Forks-E Grand Forks MPO	EAST GRAND FORKS DAR TRANSIT OPERATING ASSISTANCE		EAST GRAND FORKS	POLK	TRANSIT (TR)	LOCAL MATCH	112,500	-	-	-	112,500
TRANSIT	TRF-0018-22B	2022	L	EAST GRAND FORKS	Grand Forks-E Grand Forks MPO	SECT 5307: EAST GRAND FORKS FIXED ROUTE TRANSIT OPERATING ASSISTANCE		EAST GRAND FORKS	POLK	URBANIZED AREA FORMULA (B9)	FTA5307 (B9) LOCAL MATCH	606,200	-	135,000	-	471,200
LOCAL STREETS	119-119-013	2022	L	EAST GRAND FORKS	Grand Forks-E Grand Forks MPO	EAST GRAND FORKS, INTERSECTION OF BYGLAND ROAD & RHINEHART DRIVE, CONSTRUCT ROUNDABOUT (CAPPED \$860,000)		EAST GRAND FORKS	POLK	RC-RECONSTRUCTION	STP 5K-200K LOCAL MATCH	1,670,000	860,000	-	-	810,000
HIGHWAY MN 220	6017-44	2022	S	MNDOT	Grand Forks-E Grand Forks MPO	MN 220, NB & SB, IN EAST GRAND FORKS, SIGNAL IMPROVEMENTS AT 14TH ST, CONSTRUCT CROSSWALK AT 17TH ST AND PED RAMP IMPROVEMENT & UPGRADE SIGNAL AT MN 220/US2	0	EAST GRAND FORKS	POLK	SC-SAFETY IMPROVEMENTS	STATE TH NON-PAR LOCAL NON-PAR	600,000	-	-	448,000	152,000
HIGHWAY US 2	6001-61	2022	S	MNDOT	Grand Forks-E Grand Forks MPO	**PRS**ELLE**AB**SPP** US 2, WBL - FROM 7TH AVE NE IN EAST GRAND FORKS TO 0.3 MI E OF POLK CSAH 15 (FISHER), CRACK & BITUMINOUS OVERLAY	14.6	EAST GRAND FORKS	POLK	RS-RESURFACING	NHPP STATE TH MATCH	10,200,000	8,160,000	-	2,040,000	-
TRANSIT	TRF-0018-23A	2023	L	EAST GRAND FORKS	Grand Forks-E Grand Forks MPO	EAST GRAND FORKS DAR TRANSIT OPERATING ASSISTANCE		EAST GRAND FORKS	POLK	TRANSIT (TR)	LOCAL MATCH	115,880	-	-	-	115,880
TRANSIT	TRF-0018-23B	2023	L	EAST GRAND FORKS	Grand Forks-E Grand Forks MPO	SECT 5307: EAST GRAND FORKS FIXED ROUTE TRANSIT OPERATING ASSISTANCE		EAST GRAND FORKS	POLK	URBANIZED AREA FORMULA (B9)	FTA5307 (B9) LOCAL MATCH	620,330	-	135,000	-	485,330
TRANSIT	TRS-0018-23T	2023	L	MNDOT	Grand Forks-E Grand Forks MPO	EAST GRAND FORKS PURCHASE ONE (1) CLASS 500 REPLACEMENT BUS		EAST GRAND FORKS	POLK	TRANSIT (TR)	STBGP 5K-200K	160,000	128,000	-	-	32,000
Highway MSAS 119	60-00137	2023	A	MnDOT	Grand Forks-E Grand Forks MPO	BNSF RR, REPLACE EXISTING SIGNAL SYSTEM AT MSAS 119, 2ND AVE NE, EAST GRAND FORKS, POLK COUNTY		EAST GRAND FORKS	POLK	SR-Safety Rail	RRS LM	300,000	270,000	-	-	30,000.00
TRANSIT	TRF-0018-24A	2024	L	EAST GRAND FORKS	Grand Forks-E Grand Forks MPO	EAST GRAND FORKS DAR TRANSIT OPERATING ASSISTANCE		EAST GRAND FORKS	POLK	TRANSIT (TR)	LOCAL MATCH	119,360	-	-	-	119,360
TRANSIT	TRF-0018-24B	2024	L	EAST GRAND FORKS	Grand Forks-E Grand Forks MPO	SECT 5307: EAST GRAND FORKS FIXED ROUTE TRANSIT OPERATING ASSISTANCE		EAST GRAND FORKS	POLK	URBANIZED AREA FORMULA (B9)	FTA5307 (B9) LOCAL MATCH	639,900	-	140,000	-	499,900
TRANSIT	TRF-0018-24C	2024	L	EAST GRAND FORKS	Grand Forks-E Grand Forks MPO	SECT 5339: EAST GRAND FORKS PURCHASE ONE (1) CLASS 500 REPLACEMENT BUS		EAST GRAND FORKS	POLK	BUS AND BUS FACILITIES (BB)	FTA5339 (BB) LOCAL MATCH	180,000	-	144,000	-	36,000
HIGHWAY US 2B	6001-68	2024	S	MNDOT	Grand Forks-E Grand Forks MPO	**PRS**SPP** US 2B, IN EAST GRAND FORKS, AT 2ND ST NW & 4TH ST NW, SIGNAL SYSTEM REPLACEMENT AND ADA IMPROVEMENTS	0.2	EAST GRAND FORKS	POLK	TM-TRAFFIC MANAGEMENT	NHPP STATE TH MATCH LOCAL NON-PAR	1,300,000	653,600	-	163,400	483,000

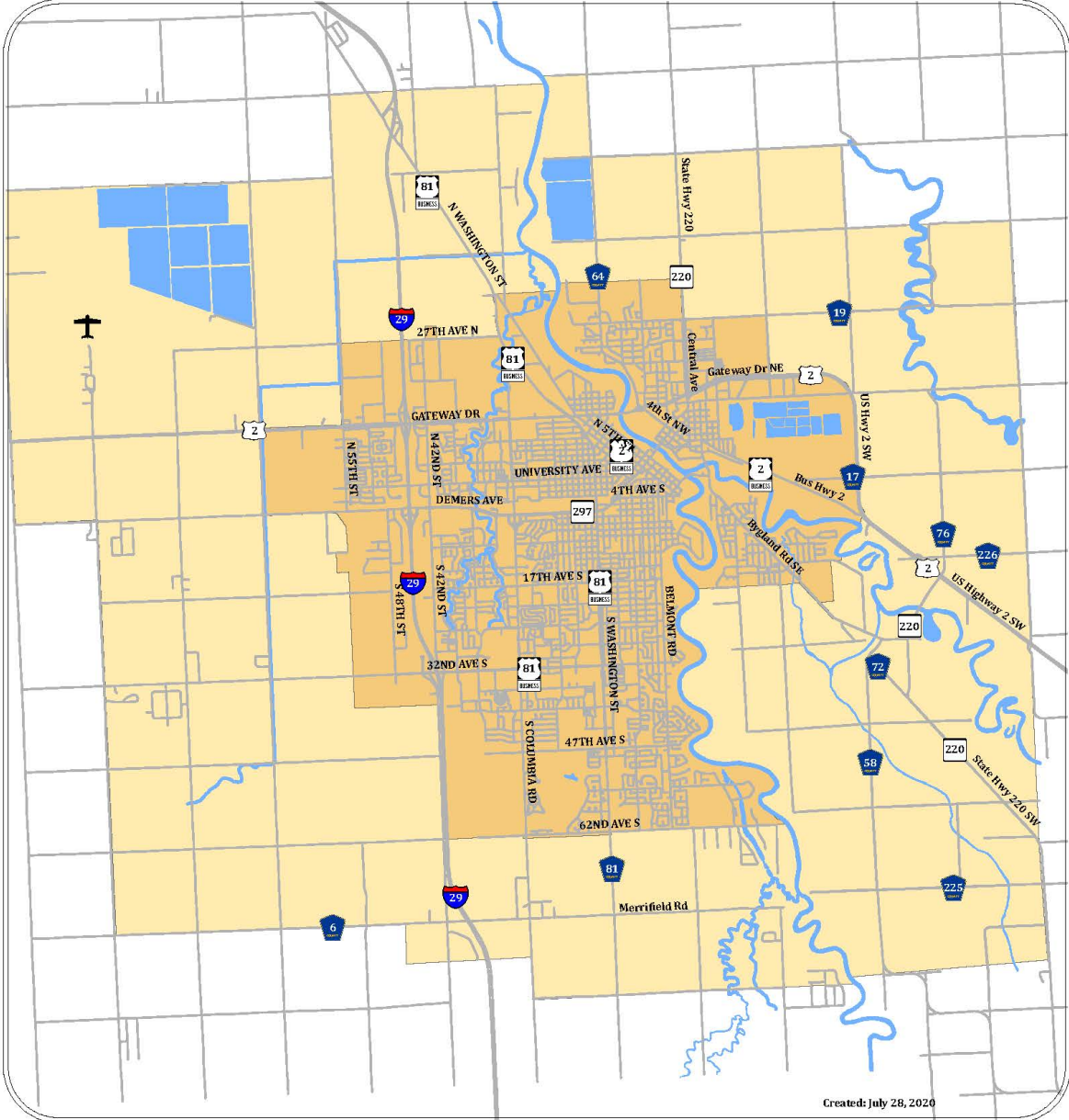
\*Projects included in the Draft ATIP within the Grand Forks/East Grand Forks MPA boundary are subject to change per the MPO TIP.



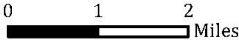
# **APPENDIX II**

## **GF/EGF MPO AREA MAP**

# Grand Forks- East Grand Forks Metropolitan Planning Organization Planning Area



- |                                     |            |                     |                |
|-------------------------------------|------------|---------------------|----------------|
| Adjusted Federal Aid Urbanized Area | Interstate | US Business Highway | County Highway |
| MPO Planning Area                   | US Highway | State Highway       | Water          |





**MPO Staff Report**  
**Technical Advisory Committee:**  
April 14, 2021  
**MPO Executive Board:**  
April 21, 2021

**RECOMMENDED ACTION: Approve Request for Quotes (RFQ) for the Pavement Conditions and Analysis Report.**

**TAC RECOMMENDED ACTION:**

Matter of Approval of RFQ for the Pavement Conditions and Analysis Report.

**Background:** The pavement condition data has been collected for the MPO since 2003 every 5 years. The last time this was done in 2013. With the collection there is an analysis report on the condition of the arterial and collector roads. After the 2015 report was finalized, the MPO was informed that we would no longer be allowed to have federal funds pay for this type of project. In 2020 the MPO was informed that we could use federal funds for this type of project. It was then put on the Work Program for 2021.

The scope of work for this project is for a data collection on arterial and collector roads in both directions within the City limits of Grand Forks and East Grand Forks. This is approximately 253 miles. Once the data is collected the pavement condition can be analyzed and a report written. This will help the MPO make recommendations for performance targets in the future, that are mandated by federal law, as well as help prioritizing projects in the 2050 Metropolitan Transportation Plan (MTP).

In the past the Cities have also done the local roads in one direction to help in deciding maintenance and preservation project priorities. This is approximately 153 miles for Grand Forks and 41 miles for East Grand Forks. The MPO has been asked to include an option for the Cities to have the local roads done as well at the Cities expense. This is being included in the RFP as well this time.

This RFQ is set to be advertised by April 23<sup>rd</sup>, with contract approval by June 16<sup>th</sup>. The final report is due by February 25<sup>th</sup>.

**Findings and Analysis**

- UPWP identifies the completion of Pavement Condition and Analysis Report.

**Support Materials:**

- Draft RFQ Scope of Work

## I. Background and Scope of Work

### A. Background

The MPO has allocated fund to pavement condition analysis. This will assist the Cities of Grand Forks and East Grand Forks in planning for future road projects. This will also assist the MPO in Federal performance measure reporting.

The MPO assisted the Cities of Grand Forks and East Grand Forks with establishing their pavement management in 2006 using ICON. The MPO continued assisting the Cities in updating their system in 2008 and 2014. In 2017 the MPO could no longer assist the Cities and the Cities took over the maintenance of the system. In 2018, the City of Grand Forks updated their pavement condition except newly build roadway. Both Cities have been inputting projects that happen on roadway segments every year at least if not when they happen. ICON is still used by both Cities.

### B. Scope of Work

This Request for Quotes (RFQ) is requesting services to evaluate pavement condition on all functionally classified collector and arterial roadways within the Grand Forks-East Grand Forks MPO. Consultants will provide data collection, assess pavement according to [CFR 490.313](#), update the jurisdictions' data programs as necessary, and provide additional analysis as requested. Data will be used by the MPO to monitor their federal performance target for pavement condition and help select preventive maintenance and/or reconstruction projects for the region. To elaborate upon the selection process, data will inform the next Metropolitan Transportation Plan update and constrained list of projects and be used, as needed, during project prioritization of the annual TIP solicitation. Also, data will help the jurisdictions to execute the rehabilitation/reconstruction of functionally classified roadways. This study will be in cooperation with the cities of Grand Forks and East Grand Forks, the North Dakota Department of Transportation (NDDOT), the Federal Highway Administration (FHWA), and the Federal Transit Administration (FTA).

This project will include data collection for the MPO's entire urban functionally classified network. The network consists of approximately 252.43 miles of collector and arterial roadway to be traveled in both directions. Approximately one-third of the area of each direction of roadway shall be collected at a time. The data shall be collected on accordance with ASTM D6433-09 and be compatible with ICON software. The data shall be compatible with the existing jurisdictional GIS systems. Additional analysis will be requested to identify jurisdictional specific repair programs, which will include timing of repair and cost.

**Option:** Local roads may be added as part of a four-way contract with the Cities of Grand Forks and East Grand Forks. Collection of local road conditions is not eligible for the MPO's federal funding. The City of Grand Forks anticipates surveying approximate 153.01 miles of local roadway in one direction. The City of East Grand Forks anticipates surveying 40.48 miles of local roadway in one direction. The Cities are expected to cost share on the projects mobilization fee at a rate matching the percentage of local roads in the total project. Any fee or expense resulting from the local road collection should be directly invoiced to the Cities.

This outline is not necessarily all inclusive. The consultant may include in the proposal additional performance tasks that will integrate innovative approaches to successfully complete the project. At a minimum, the consultant will be expected to establish detailed analyses, recommendations, and/or deliverables for the following tasks:

## **1. Project Initiation:**

- a) Development of a steering committee (with assistance from MPO staff). Steering committee should include jurisdictional staff, MPO staff, and possible state and federal oversight.
- b) Kick off meeting with steering committee.
- c) Coordination with existing geospatial and pavement management systems.

## **2. Project Management**

- a) Activities required to manage the project including staff, equipment, and documentation.
- b) Lead steering committee meetings at appropriate times throughout the project.
- c) Preparation of progress reports, documenting travel and expense receipts, and preparing and submitting invoices in a timely manner.
- d) Monthly progress reports to the MPO, the Technical Advisory Committee, and the Policy Board. The reports should be to the project manager by the end of day on the first Thursday of the month.

- 3. Georeferenced digital pavement data collection, including high resolution downward facing line-scan images and high resolution forward and side facing images at 25-foot intervals.**
- 4. Pavement evaluation according to [CFR 490.313](#).**
- 5. Pavement management software database creation and/or updates, as required, and image software installation and database creation.**
- 6. Linkage of pavement evaluation data and the digital images to existing Grand Forks and East Grand Forks respective GIS systems.**
- 7. Database import into ICON pavement management software, possible update of software license (paid at jurisdictional expense), and possible staff training in ICON.**
- 8. Analysis of pavement condition scores and development of condition reports. Depending on the need of the jurisdiction, analysis and reports may include:**
  - a) Condition report noting pavement scores as a PDF, GIS-shapefile, or similar format. Consultant should provide an ordered list of roadways to improve over the next 5-10 years. List should be based on pavement score and an assumed, generic cost estimate.
  - b) Conditions report noting pavement scores AND additional analysis on longevity of the roadways. Consultant should prioritize roadway improvements for next 5-10 years. This would note which roadway/sections to improve, the year of improvement, and the type of improvement (mill and overlay vs. reconstruction). This would use jurisdiction specific cost estimates and constrained by jurisdiction specified budget and/or condition target.
  - c) A report on overall condition rating based on the Federal pavement measures calculation for the collector and arterial roads in the MPO study area.

**9. Provide final presentations. Depending on the need of the jurisdiction, final presentations may include:**

- a) Staff level technical update robust enough for the city/MPO staff to address boards about the report. This is to be given in person or by web-based video conference.
- b) Commission presentation where the consultant presents by phone or in-person. Possible action item.
- c) In-person or video presentation to the MPO TAC and Policy Board for review and possible approval.

**10. Collection, analysis, and reporting of local road conditions for the Cities of Grand Forks and East Grand Forks.**

C. Project Deliverables

**1. Develop and Review of Draft Report:**

A draft report shall be produced after all recommendations have been developed and approved by the Steering Committee. Electronic and/or paper copies of the draft shall be provided for the steering committee, the MPO project manager, NDDOT, MnDOT, FHWA, and FTA for their review and comment. All comments from the MPO, NDDOT, MNDOT, FHWA, and FTA shall be addressed to the respective entity's satisfaction prior to development of the final draft and final presentations.

**2. Final Presentations:**

The draft report shall be advertised and made available to the public for a minimum of ten (10) days before the final presentations.

The consultant will be requested to provide a technical presentation to the staff of the jurisdictions. This is to be given in person or by web-based video conference. Additionally, they shall make a presentation to the Grand Forks City Council, East Grand Forks City Council, the MPO Technical Advisory Committee, and the MPO Policy Board for review and acceptance/approval of the final draft report. Presentation to the jurisdictional governing bodies may be given by phone, web-based video conference, or in person. Approval of the final draft report by the MPO Policy Board, and subsequent distribution of study deliverables, will mark the completion of the study.

**3. Final Deliverables:**

The final report shall be produced after all comments on the draft report are addressed, final presentations are complete, and the report has been approved by the MPO TAC and Policy Board. A minimum of six (6) paper copies of the final report shall be provided. A pdf-based and Word-based copy of the report and appendices shall be provided. An electronic

copy of all data and information collected shall be provided. All products are to be delivered to the MPO project manager for dissemination.

D. Estimated Project Budget

The MPO has a budget of \$58,000 to compensate the selected consultant to complete the scope of work as identified. Consultants submitting quotes are asked to use audited DOT rates when completing their Cost Proposal Form (See Appendix B).





**MPO Staff Report**  
**Technical Advisory Committee:**  
April 14, 2021  
**MPO Executive Board:**  
April 21, 2021

**RECOMMENDED ACTION: Recommend the approval of Agreement with East Grand Forks for FTA #5307 Funds to the MPO Executive Board.**

**TAC RECOMMENDED ACTION:**

Matter of the Memorandum of Agreement.

**Background:** As our UPWP identifies, we will be utilizing some East Grand Forks FTA #5307 program funds towards updating the Transit Development Program Element of our Metropolitan Transportation Plan.

In order for these funds to be provided, a separate agreement between East Grand Forks and the MPO is needed. MPO and City Staff, along with review by state and federal partners, have drafted the attached agreement.

We are recommending approval of this agreement. Assuming approval is granted, next month, the RFP for the TDP will be vetted through the MPO process.

**Findings and Analysis:**

- Updating the TDP has been identified in the UPWP.
- The UPWP identifies that East Grand Forks will provide some of its FTA #5307 funds.
- An agreement must be in place between East Grand Forks and the MPO.
- An agreement has been vetted through MPO and City staff with review by state/fed partners.

**Support Materials:**

- Copy of Proposed Agreement.

**CONTRACT between the City of East Grand Forks Transit and  
The Grand Forks East Grand Forks Metropolitan Planning Organization  
(GF/EGF MPO) for the  
PURPOSES OF the 2050 Transit Development Plan Update**

**THIS CONTRACT**, between the City of East Grand Forks Transit hereinafter referred to as “City Transit”, and the Grand Forks- East Grand Forks Metropolitan Planning Organization, hereinafter referred to as “Provider”, specifies the procedures, conditions and agreements between the parties for the 2050 Transit Development Plan Update for the City Transit by the provider, Grand Forks – East Grand Forks Metropolitan Planning Organization, Grand Forks ND and East Grand Forks MN.

**Recitals**

**1. Term of the Agreement**

The Provider further covenants and agrees that the Provider will commence and continue the work during the period from May 1, 2021 through December 31, 2022, and will have completed the work in every aspect to the satisfaction and approval of the City Transit.

**2. Funding**

The FTA is or will be providing federal assistance for this project in an estimated expected amount of \$120,000; the Assistance Listing number is 20.509.

**3. Agreement**

The Provider for and in consideration of the payment or payments herein specified and to be made by the City Transit, hereby covenants and agrees to furnish all materials (except such as are to be furnished by the GF-EGF MPO), all necessary tools and equipment, and to do and perform all the work and labor for the 2050 Transit Development Plan Update. The City Transit shall pay the price and compensation set forth and specified in the Provider’s Unified Planning Work Program (UPWP) signed by the Provider and hereto attached and hereby made a part of this contract. The work to be done and performed will be in accordance with the Scope of Work on file in the office of the City Transit. The Scope of Work is hereby made a part of this contract.

**4. Payment**

The City Transit agrees to pay and the Provider agrees to accept payment in accordance with the prices proposed for the unit items as set forth in the conformed copy of the Proposal and/or Quote hereto attached. The prices shall conform to those in the accepted Provider’s UPWP. Payments will be made as provided in the Scope of Work.

**5. Discrimination**

The Provider, shall not discriminate on the basis of race, color, national origin, or sex in the performance of this contract. The Provider shall carry out applicable requirements of 49 CFR Part 26 in the award and administration of DOT-assisted contracts. Failure by the Provider to carry out these requirements is a material breach of this contract, which may result in the termination of this contract or such other remedy as the City Transit deems appropriate, which may include, but is not limited to, (1) withholding monthly progress payments, (2) assessing sanctions, (3) liquidated damages, and/or (4) disqualifying the Provider from future bidding as non-responsible.

## **6. Contract Components**

The Contract consists of the following component parts, all of which are fully a part of this contract as if herein set out verbatim, or if not attached, as if hereto attached, to wit: Unified Planning Work Program (UPWP);

## **7. Minnesota Government Practices Act.**

The City Transit and the provider must comply with the Minnesota Government Data Practices Act, Minn. Stat. Ch. 13, as it applies to all data provided by the State under this agreement, and as it applies to all data created, collected, received, stored, used, maintained, or disseminated by the City Transit under this agreement. The civil remedies of Minn. Stat. § 13.08 apply to the release of the data referred to in this clause by either the City Transit or the Provider.

## **8. Termination**

**8.1 Termination.** The City Transit may terminate this agreement at any time, with or without cause, upon 30 days' written notice to the other party.

**8.2 Termination for insufficient funding.** It is understood that the validity of this contract between the City Transit and the Provider is contingent upon the receipt by the City Transit of State and Federal funding of the system during the time period specified. The City Transit may immediately terminate this agreement if it does not obtain funding State or Federal Funding, or other funding source; or if funding cannot be continued at a level sufficient to allow for the payment of the services covered here. Termination must be by written or fax notice to the Provider. The City Transit is not obligated to pay for any services that are provided after notice and effective date of termination. However, the Provider will be entitled to payment, determined on a pro rata basis, for services satisfactorily performed to the extent that funds are available. The City Transit will not be assessed any penalty if the agreement is terminated because of the decision of the State or Federal Government, or other funding source, not to appropriate funds. The City Transit must provide the Provider notice of the lack of funding within a reasonable time of the City Transit's receiving that notice.

## **9 Liability**

The Provider will indemnify, save, and hold the City Transit, its agents, and employees harmless from any claims or causes of action, including attorney's fees incurred by the City Transit, arising from the performance of this agreement by the Provider or the Provider agents or employees. This clause will not be construed to bar any legal remedies the Provider may have for the City Transit's failure to fulfill its obligations under this agreement. If the City Transit fails to enforce any provision of this agreement, that failure does not waive the provision or its right to enforce it.

## **10. Amendments**

Both parties will abide by all provisions set out within and agreed upon and detailed within the Scope of Work and Proposal/Quote and all Attachments. Any changes to the provisions agreed upon during this contract term must be modified and/or amended with a written document that is signed by both parties.

## **11. Governing Law**

The **PROVIDER** by submission of his/her UPWP assures City Transit that it shall comply with, and be bound by all applicable federal, state, and local laws, rules, and ordinances and agrees that this contract shall be governed by the laws of the State of Minnesota.

**12. Venue**

All legal proceedings out of this agreement, or its breach, must be in the appropriate state or federal court with competent jurisdiction in Polk County, Minnesota.

**13. Contract Complete.**

This agreement contains all negotiations and agreements between the State and the Governmental Unit. No other understanding regarding this agreement, whether written or oral, may be used to bind either party.

**IN WITNESS WHEREOF**, City Transit and Provider have caused this Agreement to be executed on their respective behalf as of the day and date first above shown.

Dated at \_\_\_\_\_, this \_\_\_\_\_ day of \_\_\_\_\_, YEAR.

CITY OF EAST GRAND FORKS

By \_\_\_\_\_  
Steve Gander, Mayor , and

\_\_\_\_\_  
David Murphy, City Administrator

STATE OF MINNESOTA    )  
  ) SS  
COUNTY OF POLK        )

The foregoing instrument was acknowledged before me on \_\_\_\_\_, 2021 by, Steven Gander, Mayor and David Murphy, City Administrator, for the City of East Grand Forks, Minnesota.

\_\_\_\_\_  
Notary Public  
Polk County, Minnesota  
My Commission Expires:

Approved as to form and execution this \_\_\_\_\_ day of \_\_\_\_\_, 2021.

\_\_\_\_\_  
Ronald I. Galstad, City Attorney

GRAND FORKS- EAST GRAND FORKS  
METROPOLITAN PLANNING ORGANIZATION

\_\_\_\_\_  
Jeannie Mock, Chairperson

\_\_\_\_\_  
Earl Haugen, Executive Director

STATE OF \_\_\_\_\_ )  
  ) SS  
COUNTY OF \_\_\_\_\_ )

The foregoing instrument was acknowledged before me on \_\_\_\_\_, 2021 by, Jeannie Mock, MPO Chairperson.

\_\_\_\_\_  
Notary Public  
Polk County, Minnesota  
My Commission Expires:

STATE OF \_\_\_\_\_ )  
  ) SS  
COUNTY OF \_\_\_\_\_ )

The foregoing instrument was acknowledged before me on \_\_\_\_\_, 2021 by, Earl Haugen, MPO Executive Director.

\_\_\_\_\_  
Notary Public  
Polk County, Minnesota  
My Commission Expires:

Approved as to form and execution this \_\_\_\_\_ day of \_\_\_\_\_, 2021.

\_\_\_\_\_  
[LEGAL REPRESENTATIVE AGENT]



**MPO Staff Report**  
**Technical Advisory Committee:**  
April 14, 2021  
**MPO Executive Board:**  
April 21, 2021

**RECOMMENDED ACTION: Update on Future Bridge Traffic Impact Study**

**TAC RECOMMENDED ACTION:**

Matter of the Update on Future Bridge Traffic Impact Study.

**Background:** Alliant Engineering will be participating in the TAC meeting and will provide both a recap of some items and also seek input from TAC on others. There are 4 particular items that will be discussed.

The Ad Hoc Group had its first meeting on Tuesday, April 6<sup>th</sup>. Good discussion took place with each member identifying issues, concerns, opportunities and what success might look like. A link to the video is posted as a support material. The TAC will be updated on the meeting outcome yet TAC members are highly encouraged to watch the video to benefit the most.

The second item is Alliant has drafted a document highlighting the base conditions of the Study Area. The draft is attached and we are hoping for TAC review and input. The draft doesn't contain any traffic operation analysis yet; that work is still taking place.

Third, regarding traffic operations, the scope of work indicated possible additional travel demand model runs. This anticipated major shifts in future land uses. The main reason for this work was to assist in identifying the "tipping point" of capacity that would require expansion of a possible bridge to be wider. The timing of the update of the two Cities Land Use Plans, specifically on the Grand Forks side, are not to a point where the topic of land use re-allocation has been discussed. Yet we need to conduct our traffic operations study. The approach we will be taking is to determine the adts that cause the "tipping point" and then expand the housing and employment growth into additional TAZs until the model forecast this future traffic on the bridge. This will give us some sense of the growth that would likely have to occur in order for this "tipping point" to be reached.

Fourth, the issue of school safety is an emphasis for this Study. We have asked the Safe Kids GF coalition to assist us in identifying critical Safe Routes to School crossings that will be likely impacted by traffic for each of the schools on the North Dakota side of our study area. The individual maps were distributed and members are marking up the maps and highlighting issues. These identified crossing will be observed and alternatives developed to address any concerns/issues that are revealed.

Also, Safe Kids GF will be distributing what they term “walk surveys”. This is something they do on a regular basis and were planning on doing last year but COVID change that schedule. The survey is being coordinated with the School District and will be taken during the first class of the morning by the teach finding out how many of their students stated they walked or bike to school. Another survey will be sent back with the student for their parents to complete and return to the school.

### **Findings and Analysis:**

- NONE

### **Support Materials:**

- Video of Ad Hoc Group meeting: <https://youtu.be/s283-LbD6x8>
- Draft Base Conditions Document.
- Presentation will provided at TAC meeting

## Transmittal Information

**To:** Earl Haugen (Grand Forks-East Grand Forks MPO)

**From:** Tim Burkhardt, Hannah Johnson, and Keara Pringle (Alliant Engineering)

**Date:** Revised 4/9/2021

**Subject:** Technical Memorandum #2: Existing and Future Conditions

## 1. Introduction

This is the second in a series of technical memorandums for the Grand Forks-East Grand Forks Future Bridge Traffic Impact Study. It presents the existing and future conditions as they relate to the transportation system and infrastructure, natural resources, and land use.

### 1.1 STUDY AREA

The study area comprises a roughly three mile by three mile area of Grand Forks and East Grand Forks primarily on the southern end of each city. The City of Grand Forks is in Grand Forks County, North Dakota. The City of East Grand Forks is in Polk county, Minnesota. The Red River runs between the two cities. The study corridors and intersections are listed in **Table 1-1**. The study area is illustrated in **Figure 1-1**.

**Table 1-1. Study Roadways and Intersections**

Location	Grand Forks	East Grand Forks
<b>Study Roadways</b>	○ 32 <sup>nd</sup> Avenue S	○ Bygland Road SE/3 <sup>rd</sup> Avenue SE/2 <sup>nd</sup> Avenue NE/Harley Drive
	○ 24 <sup>th</sup> Avenue S	○ US 2
	○ Elks Drive	○ Rhinehart Drive
	○ Demers Avenue	○ TH 220
	○ 4 <sup>th</sup> Avenue	○ 1 <sup>st</sup> Street SE
	○ S/Minnesota Avenue	
	○ Washington Street	
	○ Cherry Street	
	○ Belmont Road	
	<b>Study Intersections</b>	○ 32nd & Washington
○ 32nd & Cherry		○ Bygland Road SE (CR 72) & Rhinehart Drive
○ 32nd & Belmont		○ Rhinehart Drive & Greenway Boulevard
○ 24th & Washington		○ Rhinehart Drive & Future 24 <sup>th</sup> Street SE
○ 24th & Cherry		○ Rhinehart Drive & Future 32 <sup>nd</sup> Street SE
○ 24th & Belmont		○ Bygland Road SE (CR 72) & Greenway Boulevard
○ Belmont & Elks Dr		○ Bygland Road SE (CR 72) & Bygland Road SE/190 <sup>th</sup> Street SW
○ Demers & Washington		○ TH 220 & Harley Drive
○ 4th & Cherry		○ TH 220 & US 2
○ 4th & Belmont		

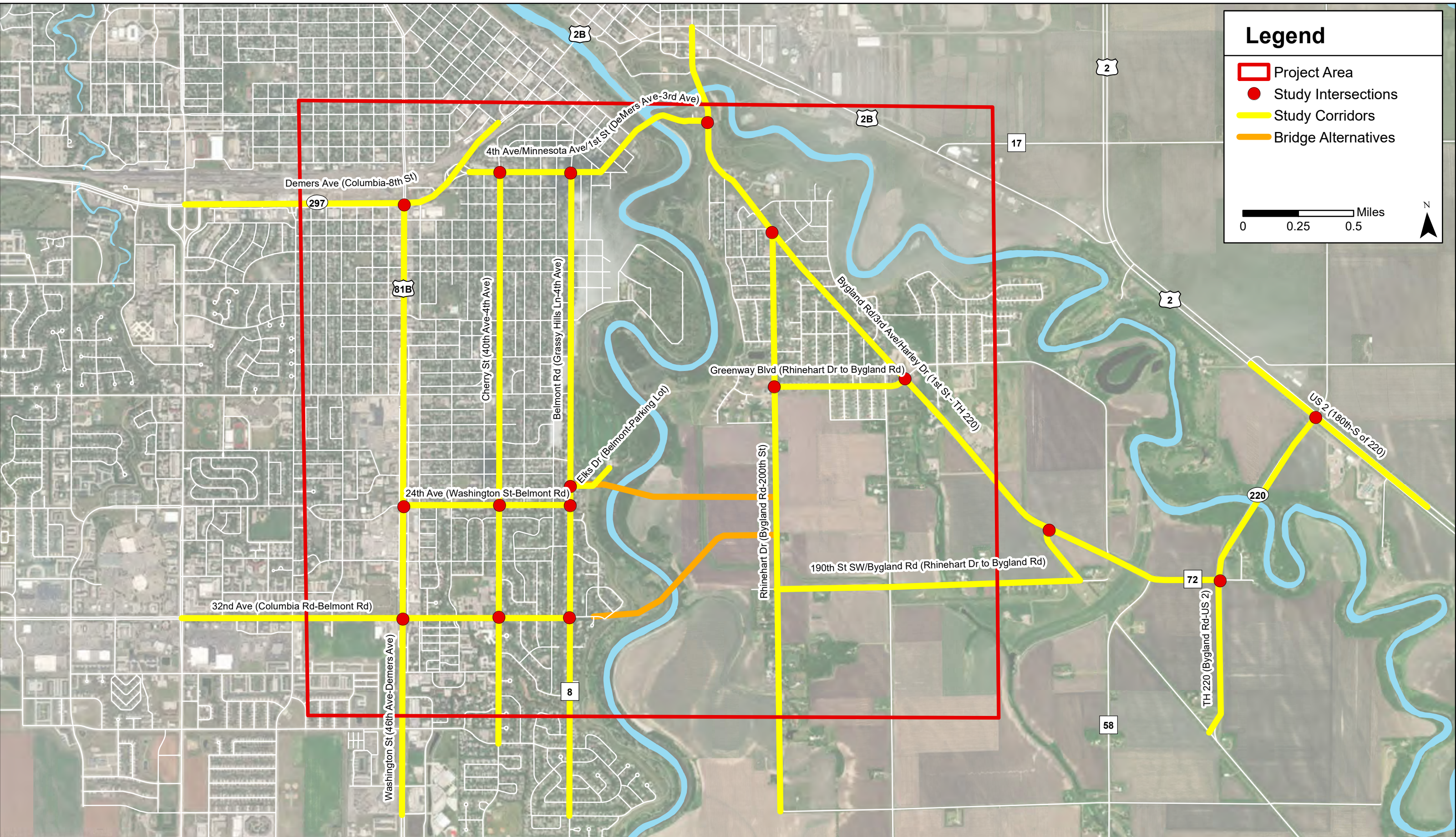


### Legend

- Project Area
- Study Intersections
- Study Corridors
- Bridge Alternatives

0 0.25 0.5 Miles

N



Future Bridge Corridor Study



*Figure 1-1  
Study Area*

Source: ESRI World Imagery Basemap

## 1.2 STUDY PURPOSE

The Alliant Engineering, Inc. (Alliant) team is supporting the Grand Forks-East Grand Forks Metropolitan Planning Organization (MPO) and local, state, and federal partners to conduct a traffic impact study of a future bridge between Grand Forks, ND, and East Grand Forks, MN, across the Red River.

Prior studies and plans have identified the need for a new local river crossing between the two cities to reduce congestion at the existing crossings and the surrounding roadway network. These include the 2018 Metropolitan Transportation Plan and the 2020 Hydraulic Analysis of South End Red River Bridge study. The conclusions of these studies support further analysis of a new river crossing in the corridors: Elks Drive and 32<sup>nd</sup> Avenue. The current study will develop and evaluate river crossing alternatives in these corridors and related improvements on the local street system as needed. .

## 1.3 STUDY APPROACH

The project will be conducted in three phases: Discovery, Development, and Design. Study content as it relates to the phase and goals is summarized in **Table 1-2**. This existing and future conditions analysis falls into the “Discovery” phase.

**Table 1-2. Study Phases and Goals**

Phase and Goals	Study Tasks
<p><b>Discovery</b>  <i>Discover and understand existing and future conditions, constraints, and stakeholder needs and concerns.</i></p>	<ul style="list-style-type: none"> <li>• Study purpose and goals</li> <li>• Existing and future conditions</li> <li>• Project needs and benefits</li> </ul>
<p><b>Development</b>  <i>Develop and evaluate potential crossing and traffic options that respond to identified issues and needs.</i></p>	<ul style="list-style-type: none"> <li>• Alternatives</li> <li>• Evaluation results</li> </ul>
<p><b>Documentation</b>  <i>Summarize the study results, identify phasing, and set up the project for the next step</i></p>	<ul style="list-style-type: none"> <li>• Recommendations</li> <li>• Phasing</li> <li>• Study report</li> </ul>

## 2. Existing Transportation System and Infrastructure

Key components of the existing transportation system and infrastructure relevant to the Future Bridge Traffic Impact Study include roadway characteristics, infrastructure, public/private access, and multimodal characteristics. These features and conditions are documented in the following sections.

### 2.1 ROADWAY CHARACTERISTICS

The following sections define the key roadway characteristics including the functional classification, roadway geometrics, traffic control devices, parking, and right of way.

#### 2.1.1 Functional and Funding Classification

Roadways serve two major functions: access and mobility. The function of a roadway is dependent on its classification. Interstates and principal arterials provide the highest degree of mobility but are limited in providing land access. Local streets provide a high degree of land access with less mobility. **Figure 2-1** shows a comparison of the different functional classifications relating access to mobility.



**Figure 2-1. Access and Mobility Relationship to Functional Classification**

The study roadways are classified as detailed in **Table 2-1**. **Figure 2-2** shows the transportation system functional classification of the roadway network.

**Table 2-1. Study Corridor Functional Classification**

Corridor	Functional Classification
<b>32<sup>nd</sup> Avenue S</b>	Principal Arterial west of Washington Street Minor Arterial east of Washington Street
<b>24<sup>th</sup> Avenue S</b>	Collector
<b>Elks Drive</b>	Local Road
<b>Demers Avenue</b>	Principal Arterial
<b>4<sup>th</sup> Avenue S/Minnesota Avenue/ 1<sup>st</sup> Street SE</b>	Minor Arterial
<b>Washington Street</b>	Principal Arterial
<b>Cherry Street</b>	Collector
<b>Belmont Road</b>	Minor Arterial
<b>Bygland Road SE/3rd Avenue SE/ 2nd Avenue NE/Harley Drive</b>	Minor Arterial
<b>US 2</b>	Principal Arterial
<b>Rhinehart Drive</b>	Major Collector north of Greenway Blvd Minor Collector Between Greenway Blvd and 182 <sup>nd</sup> Street SW Local Road south of 182 <sup>nd</sup> Street NW
<b>TH 220</b>	Minor Arterial

### 2.1.2 Roadway Access Inventory

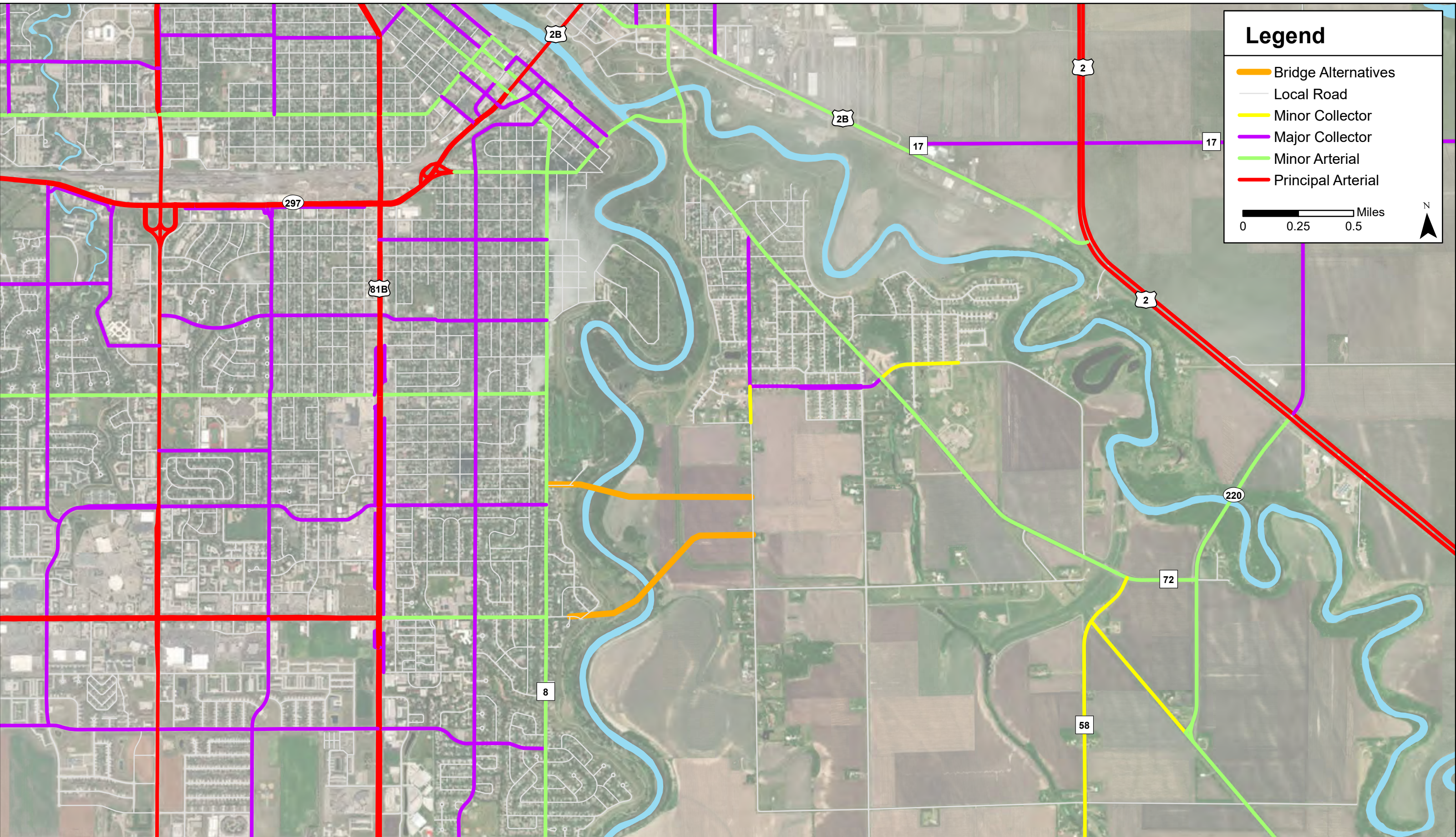
Access is a key factor affecting the quality of roadway mobility and safety . An illustration of each public access point and non-residential driveway along the study corridors is provided in **Figure 2-3**.

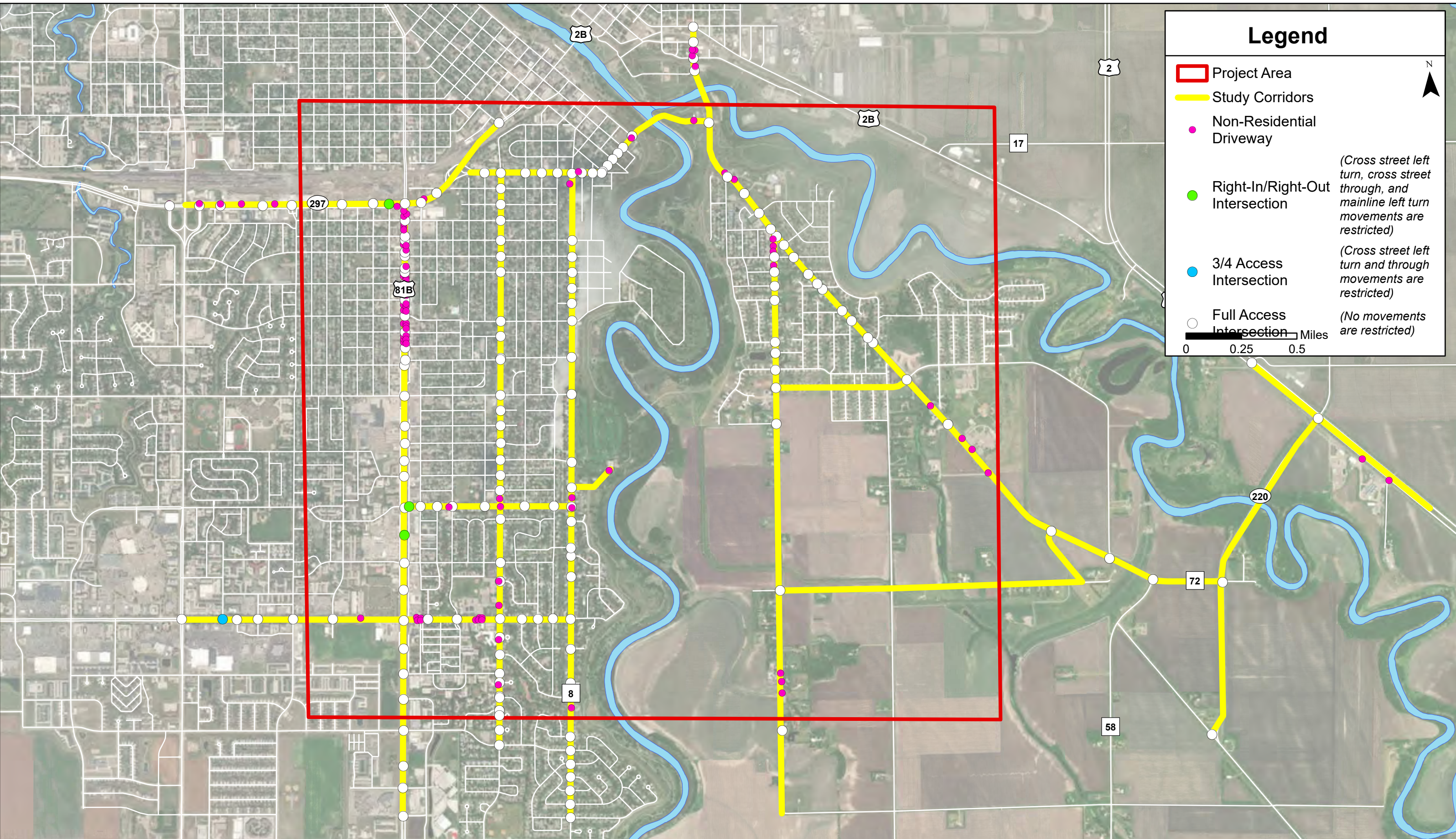
### Legend

- Bridge Alternatives
- Local Road
- Minor Collector
- Major Collector
- Minor Arterial
- Principal Arterial

0 0.25 0.5 Miles

N





**Figure 2-3**  
*Existing Access Inventory and Classification*

Source: ESRI World Imagery Basemap

### 2.1.3 Lane Geometrics, Traffic Control, and Typical Sections

Roadway typical sections vary within the study area. The following general characteristics are present:

- **32nd Avenue S:**
  - Four lane divided urban design roadway with left and right turn lanes west of Washington Street. No on-street parking.
  - Two lane urban residential east of Washington Street. Parking is generally allowed on both sides.
- **24th Avenue S:**
  - Two lane urban residential design. Parking is generally allowed on both sides.
- **Elks Drive:**
  - Two lane urban residential with no lane markings. Parking is allowed on the south side.
- **Demers Avenue:**
  - Four lane divided urban design roadway with left and right turn lanes southwest of 4<sup>th</sup> Avenue S. No on-street parking.
  - Four lane undivided urban design roadway with minimal access northwest of 4<sup>th</sup> Avenue S. No on-street parking.
- **4th Avenue S/Minnesota Avenue/1<sup>st</sup> Street SE:**
  - Three lane (two lanes westbound, one lane eastbound) urban residential west of the alley behind the Grand Forks Senior Center driveway. Parking is allowed on the south side.
  - Two lane urban residential with turn lanes at key intersections east of the alley. Parking is allowed on the south side west of the Point Bridge. Along the bridge and to the east there is no parking.
- **Washington Street:**
  - Five lane (two lanes each direction with a two-way center left turn lane) urban design north of Hammerling Avenue. No on-street parking.
  - Four lane divided urban design roadway with left and right turn lanes South of Hammerling Avenue. No on-street parking.
- **Cherry Street:**
  - Two lane urban residential design. Parking is generally allowed on both sides south of 17<sup>th</sup> Avenue. Between 17<sup>th</sup> Avenue and 10<sup>th</sup> Avenue parking is generally allowed on the west side. No on-street parking north of 10<sup>th</sup> Avenue.
- **Belmont Road:**
  - Two lane urban residential design. Parking is generally allowed on both sides south of 32<sup>nd</sup> Avenue. Parking is allowed on the west side north of 32<sup>nd</sup> Avenue.
- **Bygland Road SE/3rd Avenue SE/2nd Avenue NE/Harley Drive:**
  - Two lane roadway with turn lanes at key locations, transitions between rural on the eastern end and urban on the western end. No on-street parking southeast of Greenway Boulevard. Parking is generally allowed on both sides northwest of Greenway Boulevard.
- **US 2:**
  - Four-lane divided rural roadway design with turn lanes at key intersections and driveways. No on-street parking.
- **Rhinehart Drive:**

- Two lane residential design, transitions between rural on the southern end and urban on the northern end. Parking is generally allowed on both sides north of 13<sup>th</sup> Street SE. No on-street parking south of 13<sup>th</sup> Street SE.
- **TH 220:**
  - Two lane rural design. No on-street parking.
- **Greenway Boulevard and 190<sup>th</sup> Street data will be added in next revision**

**Figure 2-4** illustrates the key roadway lane geometrics and traffic control devices.

#### 2.1.4 Right of Way, Above Ground Utilities, and Street Lighting

Right of way was estimated using parcel mapping was provided by the City of Grand Forks and Polk County. Right of way varies by corridor. Above ground utilities in the corridor consist primarily of electric transformer pads and drainage structures and features. Transmission power lines exist along the following corridors:

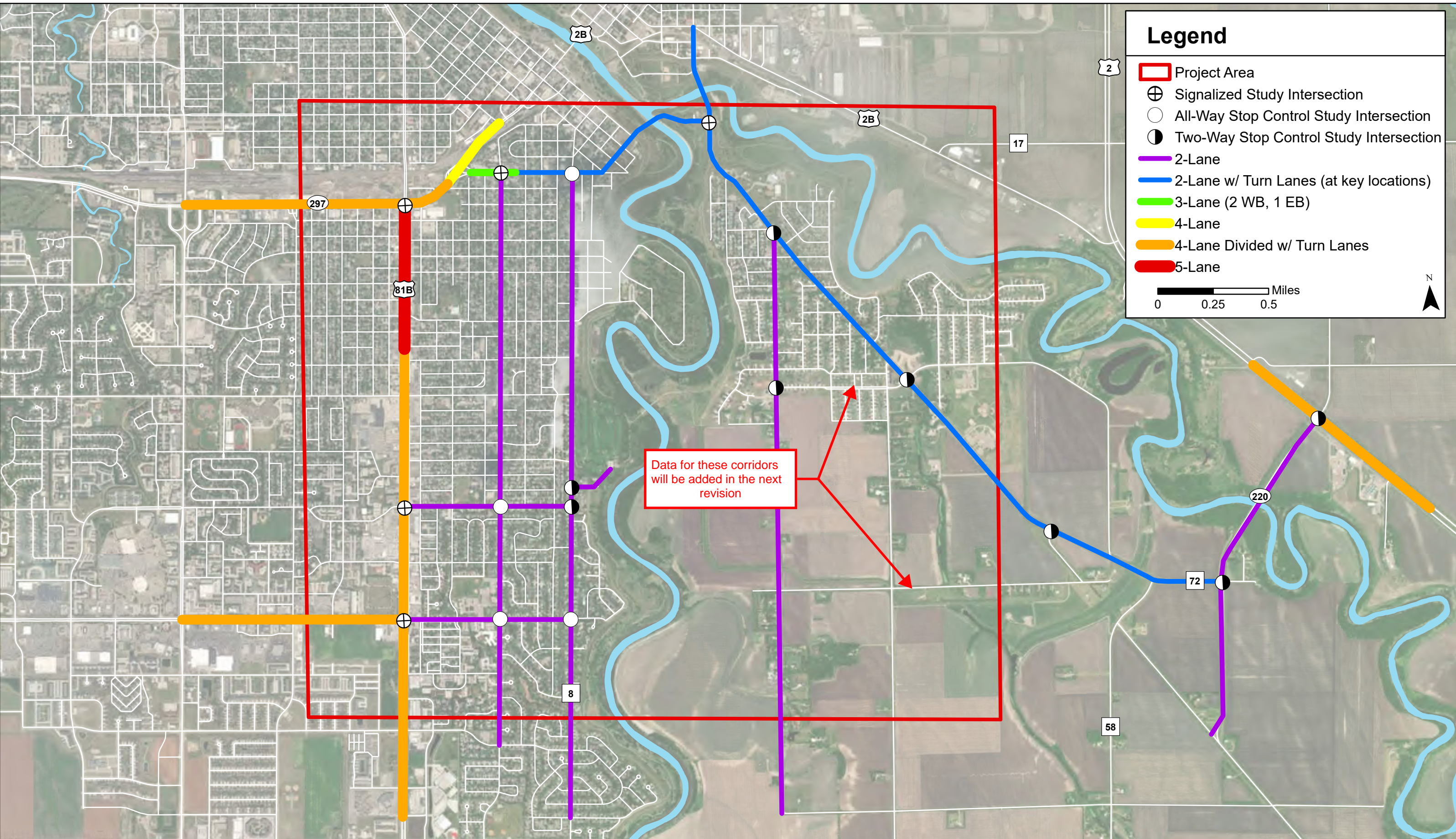
- 32<sup>nd</sup> Avenue S, east of Washington Street (along the south side)
- 24<sup>th</sup> Avenue S, ½ block west of Belmont Road (along the south side)
- Demers Avenue, between S 24<sup>th</sup> Street and S 20<sup>th</sup> Street (along the north side)
- Washington Street, south of 32<sup>nd</sup> Avenue S (along the west side)
- Belmont Road, south of 32<sup>nd</sup> Avenue S and between 27<sup>th</sup> and 13<sup>th</sup> Avenues S (along the east side)
- Rhinehart Drive, south of 182<sup>nd</sup> Street SW (along the west side)

Street lighting is provided along most corridors and at major intersections. **Figure 2-5** illustrates the estimated right of way (based on property parcels) and lighting.

#### 2.1.5 Pavement Conditions

Based on visual inspection, pavement conditions on the study corridors appear to be acceptable. There is no pavement on the southern end of Rhinehart Drive SE for roughly 0.10 miles. There are some pavement patches on Rhinehart Drive SE, Belmont Road, 24<sup>th</sup> Avenue S, and the eastbound lanes of US 2. There is heavy patching on 32<sup>nd</sup> Avenue east of S 10<sup>th</sup> Street. A detailed review of pavement conditions is beyond the scope of this memo. However, further review may be undertaken if needed during the alternatives development process.



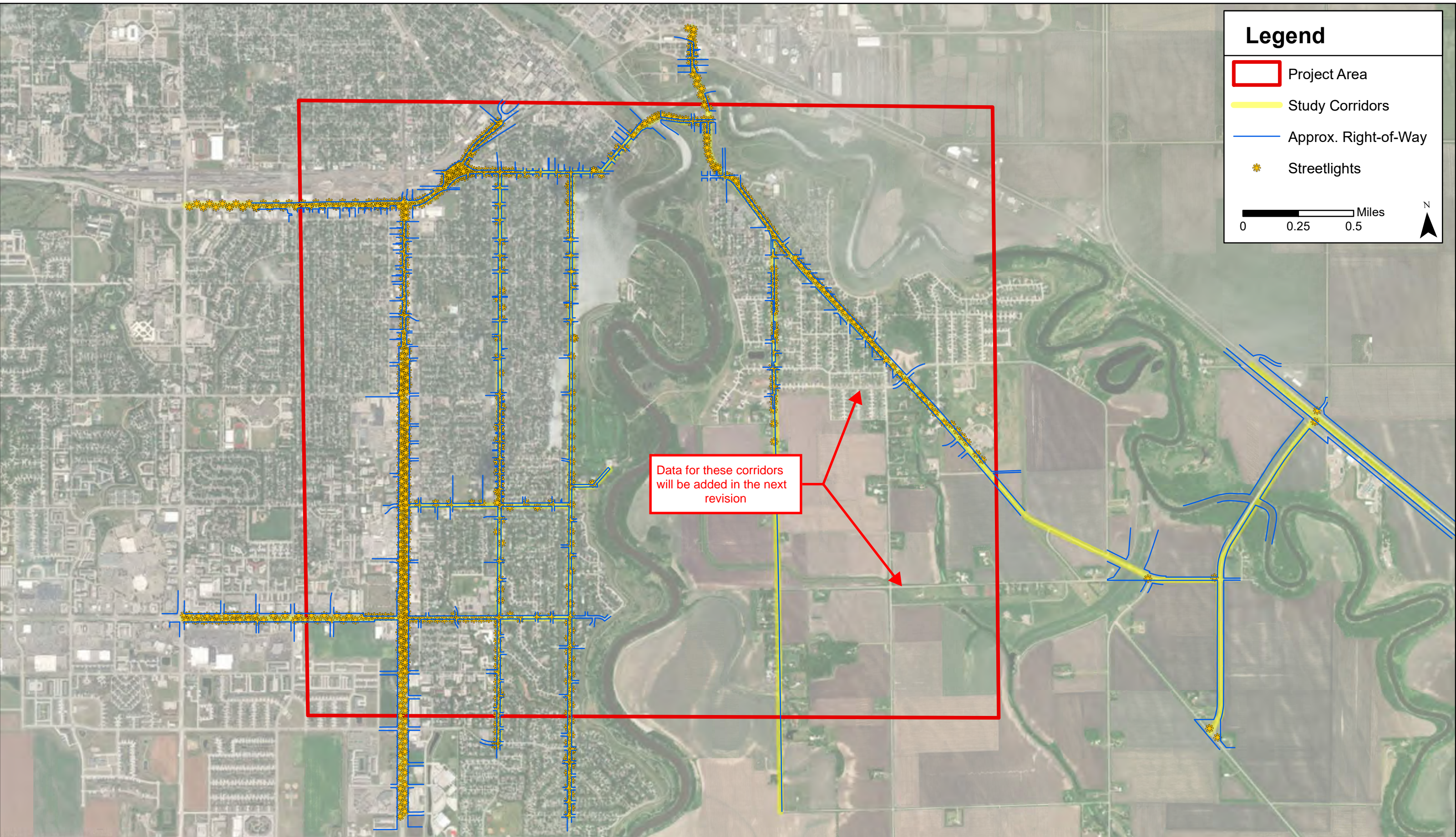


**Legend**

- Project Area
- Study Corridors
- Approx. Right-of-Way
- \* Streetlights

0 0.25 0.5 Miles

N



### 2.1.6 Structures

There are six bridges connecting Grand Forks and East Grand Forks over the Red River: one railroad bridge, two pedestrian-only bridges, and three bridges accessible by vehicles. The railroad bridge, one pedestrian bridge, and the US Hwy 2 bridge are in the study area. Each of the bridges connecting the cities are detailed below:

- **Point Bridge**

The Point Bridge is the southernmost connection between Grand Forks and East Grand Forks. It connects Minnesota Avenue in ND and 1<sup>st</sup> Street SE in MN. Access is restricted to vehicles.

- **Sorlie Bridge (not in study area)**

The Sorlie Bridge connects each of the two cities' downtown areas via Demers Avenue. There is a sidewalk on either side of the corridor.

- **Kennedy Bridge (not in study area)**

The Kennedy Bridge is the northernmost connection between the cities via US Highway 2. There is a sidewalk on the north side of the corridor.

- **BNSF Railroad Bridge**

The railroad bridge between Grand Forks and East Grand Forks is located between the Point Bridge and Sorlie Bridge.

- **Pedestrian Bridges**

There are two pedestrian bridges connecting Grand Forks and East Grand Forks. They are both part of the Greenway trail system. Only one is within the project study area.

- The southern bridge is near 17<sup>th</sup> Avenue S in Grand Forks and Laurel Drive SE in East Grand Forks.
- The northern bridge (not in study area) is near Red Dot Place in Grand Forks and 20<sup>th</sup> Street NW in East Grand Forks.

## 2.2 MULTIMODAL CHARACTERISTICS

The following sections document the key features of the pedestrian, bicycle and transit systems in the study area.










### 2.2.1 Sidewalk, Trails, Bike Lanes, Shared-Use Paths

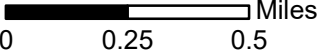
Sidewalk or multiuse trails exist along most the study roadways. The Grand Forks-East Grand Forks 2045 Metropolitan Transportation Plan (MTP) identifies plans for additional bicycle and pedestrian improvements along many corridors in each city. **Figure 2-6** illustrates the existing and planned future sidewalk, trails and bicycle facilities as documented in the 2045 MTP.


### 2.2.2 Transit Facilities

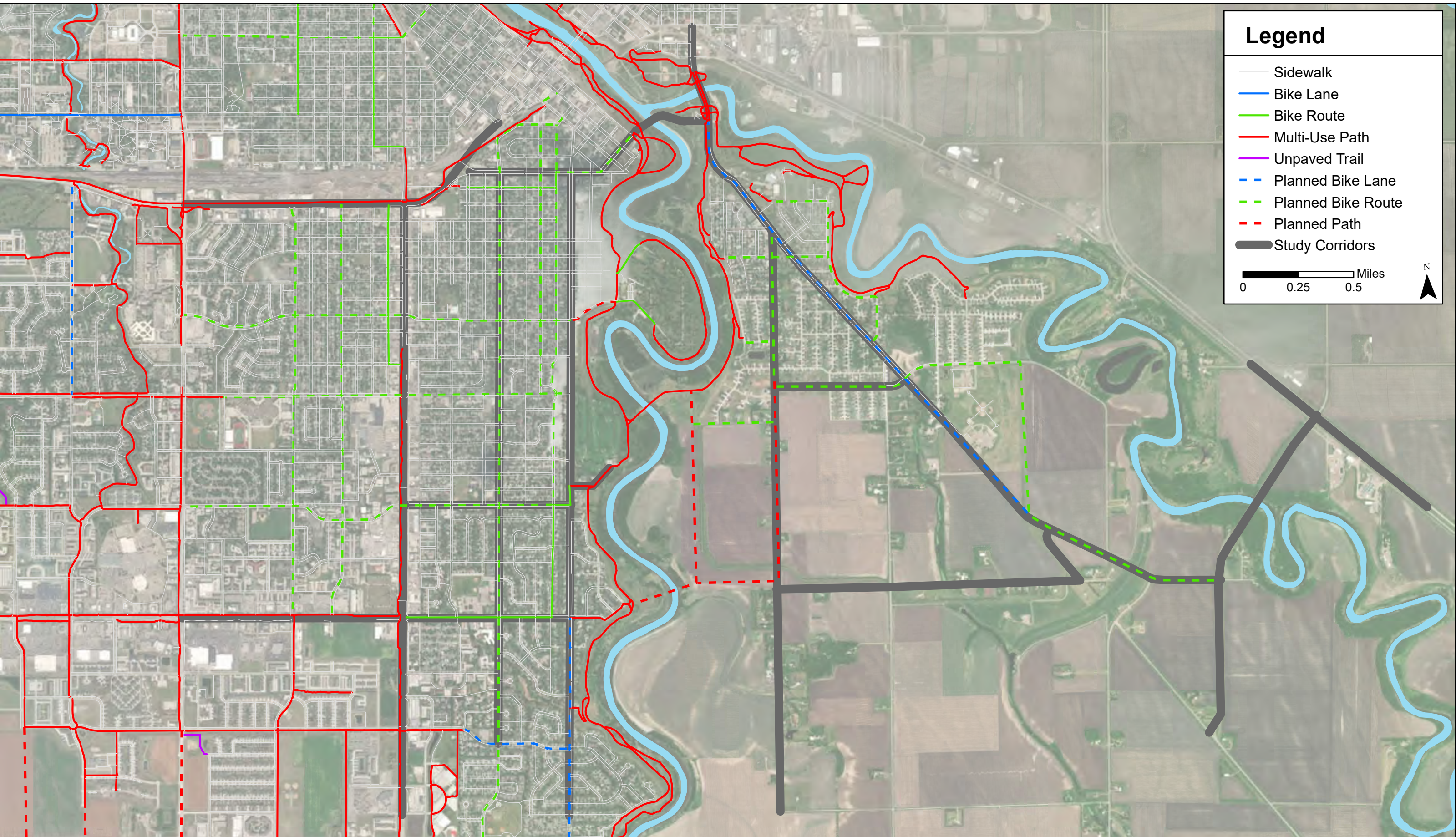
Cities Area Transit (CAT) is the public transportation system serving Grand Forks and East Grand Forks metropolitan area. **Figure 2-7** illustrates the CAT network in and around the study area.

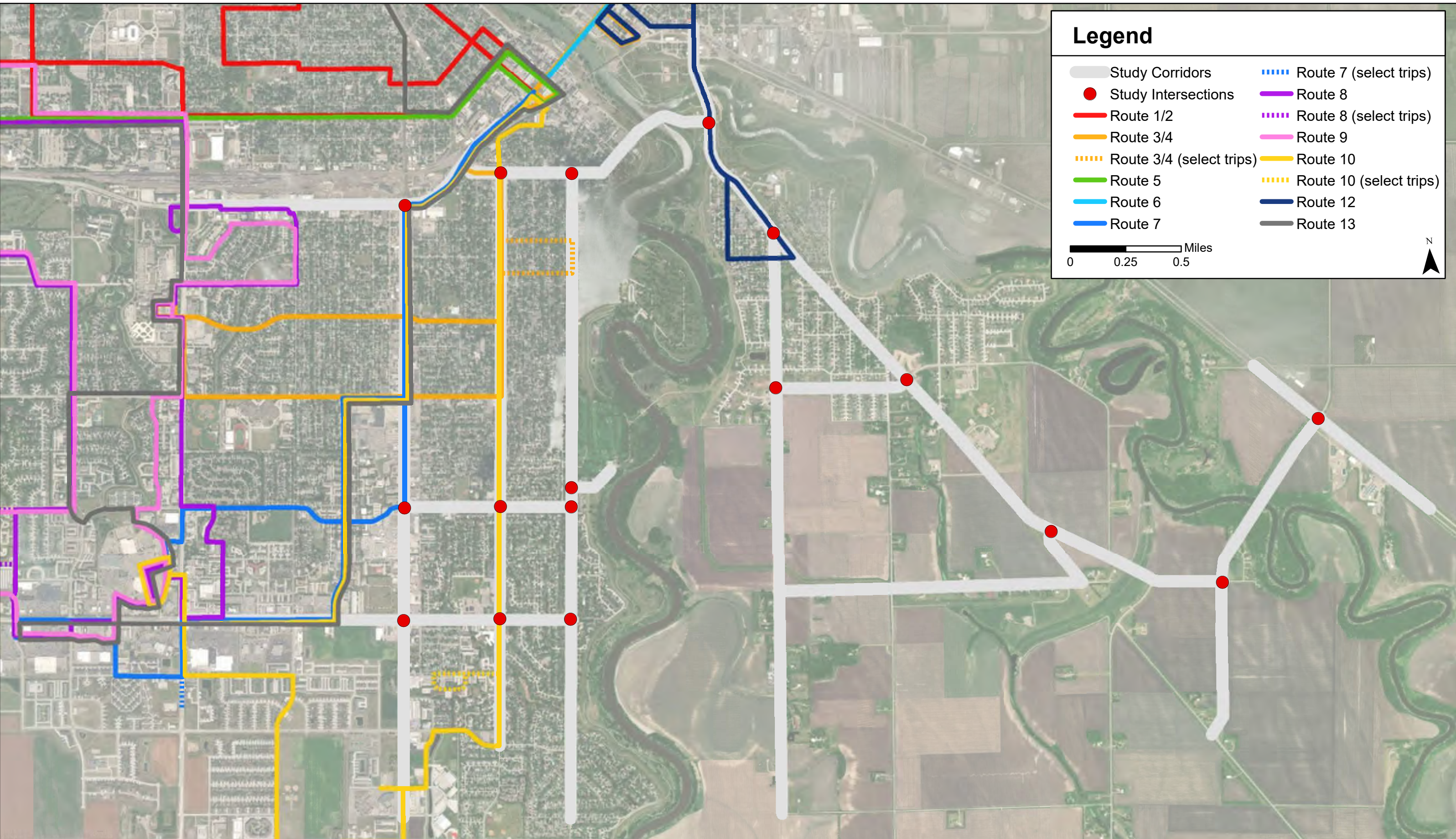
### Legend

-  Sidewalk
-  Bike Lane
-  Bike Route
-  Multi-Use Path
-  Unpaved Trail
-  Planned Bike Lane
-  Planned Bike Route
-  Planned Path
-  Study Corridors

 Miles  
0 0.25 0.5

 N





Future Bridge Corridor Study



Figure 2-7  
Transit Network

Source: ESRI World Imagery Basemap

## 3. Built and Natural Environment

A review of the existing and planned future conditions was completed within the future bridge study corridors. The purpose of this review is to identify issues and resources that should be considered at a screening level because they might differentiate one bridge location from another or because they might present a fatal flaw. This review is being conducted consistent with a Planning and Environmental Linkages (PEL) study and not at the level of detail for formal environmental documentation (National Environmental Policy Act or NEPA), which will be a subsequent step in this process.

The key elements included in the study scope of work were reviewed as documented below. Items were excluded from the analysis if they were not considered relevant to this initial review. These include relocations, energy, and temporary construction. If relevant, these items could be reconsidered during the evaluation process.

### 3.1 COMMUNITY RESOURCES

#### 3.1.1 Environmental Justice

The Grand Forks-East Grand Forks Metropolitan Planning Organization (MPO) Environmental Justice Program Manual outlines the procedures for delineating the presence of environmental justice populations within a study area. Based on this guidance, areas of minority and poverty groups have been identified as occurring within the Grand Forks portion of the study corridor near Cherry Street, S Washington Street, and 4<sup>th</sup> Avenue South (**Figure 3-1**). During the planning phase and the public involvement of this project, the project will aim to involve these groups and ensure they receive the analysis on the benefits and impacts of the proposed bridge alternatives.

#### 3.1.2 Schools

There are eight schools highlighted within the study area that include elementary, middle, and high schools (**Figure 3-1**). On the Grand Forks side, there are six schools within the study area and on the East Grand Forks side, there are two schools. Due to the close proximity to the bridge alternatives, particularly on 4<sup>th</sup> Avenue S, 24<sup>th</sup> Avenue S, and 32<sup>nd</sup> Avenue S, the Grand Forks schools could see an increase in traffic near the schools. With the increase in traffic, it is important to ensure there are safe routes and access to school. Both Grand Forks and East Grand Forks, the School Districts, the MPO and Safe Kids Grand Forks have developed Safe Routes to Schools maps for schools in the study area and are actively monitoring and updating school walking routes and issues in relation to this study. In addition, ongoing discussions regarding possible consolidation and redistricting for Grand Forks schools will be incorporated into the study as relevant.

#### 3.1.3 Historic and Cultural Resources

Both the Minnesota and the North Dakota State Historic Preservation Offices (SHPO) should be consulted during the planning stages of this project to determine if any known historic and cultural resources exist within the study corridor. The SHPO consultation will help determine where the archaeological and historic sites in relation to the study corridor and what measures need to be taken to preserve these areas.

Grand Forks has at least two historic neighborhoods, the Near Southside Historic District and the Downtown Grand Forks Historic District, that could be affected by the projected (**Figure 3-1**). These areas carry historic protections and the community members are passionate about their preservation, including from traffic impacts. The Near Southside neighborhood in particular today bears a disproportionate traffic burden related to river crossing traffic, setting up an equity conversation that will be important to address. Additionally, the Grand Forks Historical Society is located on Belmont Road just south of Elks Drive and is identified as a Section 106 property.

#### 3.1.4 Parks, Open Space, and Recreational Areas

---

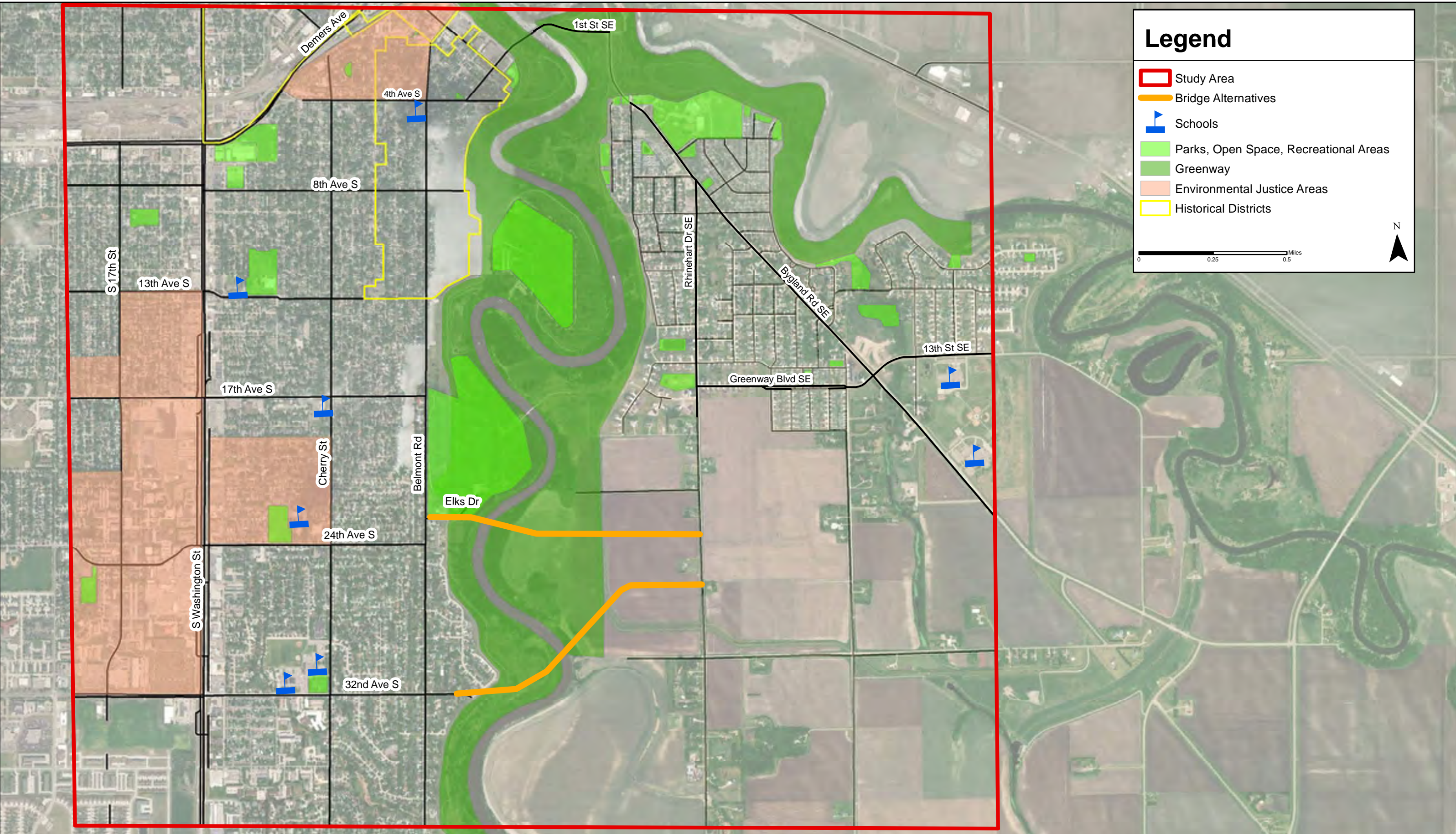
The majority of the parks, open space, and recreational areas within Grand Forks and East Grand Forks occur along the Red River and Red Lake River as part of the Greenway system (**Figure 3-1**). The Greenway system extends north to south along the Red River and east to west along the Red Lake River. There are other designated park areas within both cities that are outside the Greenway system.

The Greenway system, parks, and paths for walking and bicycling is a vital part of the Grand Forks and East Grand Forks community. It is important that bridge alternatives preserve the natural features within the Greenway system and maintain a sufficient park and trail system as outlined in both cities' land use plans.

#### 3.1.5 Pedestrian/Bicyclist

---

Pedestrian and bicycle facilities are discussed and documented in a prior section of this memo.



*Figure 3-1  
Community Resources*



## 3.2 NATURAL RESOURCES

### 3.2.1 Water Quality

Currently, there are vehicular bridges that connect the downtown areas of Grand Forks and East Grand Forks and there is a pedestrian bridge in the study area near 17<sup>th</sup> Avenue South in Grand Forks as discussed in section 2.1.6. The proposed bridge alternatives would introduce a new river crossing that does not exist today. Short term impacts to water quality can be anticipated during the construction of the bridge; however, long term impacts to water quality are not anticipated.

In order to mitigate the short term impacts to water quality, the project proponent will be required to obtain a NPDES/SDS General Permit prior to the construction of the project since the project will likely disturb more than one acre of land. BMPs will be required to be installed during construction to reduce erosion and sediment loading into the surrounding water resources. To confirm that the BMPs are effectively working, the BMPs will be inspected per the requirements of the Storm Water Pollution Prevention Plan (SWPPP). A complete list of BMPs will be described in the SWPPP that would be prepared for the project prior to construction.

### 3.2.2 Wetlands

The United States Fish and Wildlife (USFWS) Service National Wetlands Inventory (NWI), the Minnesota Department of Natural Resources (DNR) Wetland Inventory, and the MN DNR Public Waters Inventory was utilized to explore the presence of wetlands within the study area. The USFWS NWI and MN DNR NWI identified approximately 75 wetlands within the study area (**Figure 3-2**). The MN DNR Public Waters Inventory did not identify any public water wetlands within the study area. The majority of these wetlands are adjacent to the Red River and the Red Lake River. These wetlands include freshwater emergent, shrub, forested, pond, and riverine wetlands.

During subsequent project development efforts, a field wetland delineation should be conducted before the construction of the proposed project to determine wetland size and type present within the construction limits. The state regulatory authority for Minnesota and North Dakota who administers the state wetland regulations and the Army Corps of Engineers shall provide approval for any wetland delineation and permitting plans that are associated with this project.

### 3.2.3 Water Body Modification, Wildlife, Invasive Plant Species

#### ***Water Body Modification***

This study assumes a new bridge will span across the Red River south of downtown Grand Forks/East Grand Forks. See **Figure 3-2**. When an alternative is selected and the design of the bridge is finalized, appropriate Army Corps and DNR permits should be obtained to conduct work in the Red River.

#### ***Wildlife***

Wildlife present in the study corridor consist of common wildlife adapted to urban and agriculture environments, such as white-tailed deer, songbirds, and small mammals (squirrels, rabbits, raccoons). During construction of the bridge, mobile wildlife present within the project site will likely disperse to adjacent and/or similar habitats and less mobile species may likely experience more adverse effects from construction.

However, once construction is completed, the area below the bridge will be restored to previous conditions where appropriate, allowing the wildlife species back into this habitat.

### ***Invasive Plant Species***

Invasive plant species have an impact on agriculture, native plant communities, and the natural environment. It is not known if any invasive plants are present within the study area. During construction, efforts should be made to prevent the propagation and spread of invasive plant species. Prior to any construction activity, a noxious weed survey should be conducted to determine the presence and extent of any plants listed on the Minnesota and North Dakota Noxious Weed List. If present, a noxious weed plan should be developed that outlines specific eradication plans for each species present and guidelines for the prevention of spreading of seed and plant materials during construction.

When there is work within the Red River, all equipment should be decontaminated before it is put into the river and when the equipment is taken out of the river. This will prevent aquatic nuisance species from being transported to other waterways and negatively impacting them.

### 3.2.4 Floodplain

The Red River flows south to north within the study area. The Federal Emergency Management Agency (FEMA) has mapped the existing floodplain associated with the river (**Figure 3-2**).

Proposed changes within the floodway area will require close coordination and appropriate approvals obtained with FEMA, the Army Corps, and Grand Forks and East Grand Forks floodplain manager during project development.

#### 3.2.4.1 Flood Protection Infrastructure

The existing flood protection system on both the Grand Forks and East Grand Forks side is highlighted in **Figure 3-2**. Within the study corridor, Grand Forks flood protection infrastructure includes a flood protection wall and earth levee and East Grand Forks includes a earth levee. In Grand Forks, there is a stop log opening that exists for Elks Drive and on 32<sup>nd</sup> Avenue South there is an opening primarily for pedestrians/bicyclists and the pump station. On the East Grand Forks side, there are some existing openings that could be located near the proposed bridge alignments. For the proposed 32<sup>nd</sup> Avenue and Elks Drive new river crossing areas, it will be necessary to identify where the flood control infrastructure is located in relation to the bridge alignment and avoid or mitigate unnecessary adverse impacts to existing flood control infrastructure.

### 3.2.5 State Scenic River

The Red River and Red Lake River are located within the project study area and they are not designated Wild and Scenic Rivers by Minnesota and North Dakota.

### 3.2.6 Threatened and Endangered Species

#### **State Level**

The National Heritage Information System is managed by the Minnesota Department of Natural Resources (DNR) and identifies the State's rare plant, animal, and native plant communities, and other rare features. Rare species tracked within the NHIS include sightings of State endangered, threatened, or special concern

species as well as Federally listed threatened and endangered species. The NHIS data should be requested for the East Grand Forks area to understand what state listed species are within the study corridor.

In North Dakota, there is no state threatened and endangered species list and North Dakota relies on the species list identified in the U.S. Fish and Wildlife (USFWS) Endangered Species Act. The USFWS office in North Dakota has primary oversight over the threatened and endangered species here. The federally listed species below can be relied upon to identify the listed species in Grand Forks.

### Federal Level

The Information for Planning and Consultation (IPaC) is a U.S. Fish and Wildlife Service (USFWS) online project planning tool which streamlines the USFWS environmental review process. The online tool was utilized to determine if any Federally listed species, critical habitat, migratory birds, or other natural resources may be impacted by the project.

The following Federally listed species were identified as potentially occurring within the project area. While these species may potentially be affected by the future project, no critical habitat for these species exists within the study area.

- Federally endangered
  - Whooping crane (*Grus americana*)
  - Poweshiek Skipperling (*Oarisma poweshiek*)
- Federally threatened
  - Northern long-eared bat (*Myotis septentrionalis*)
  - Dakota skipper (*Hesperia dacotae*)

Additionally, there are several migratory birds listed below that are of particular concern either because they occur on the USFWS Birds of Conservation Concern (BCC) list or warrant special attention in the project area. These include:

- Species of Conservation Concern
  - American bittern (*Botaurus lentiginosus*)
  - Black tern (*Chidonias niger*)
  - Black-billed cuckoo (*Coccyzus erythrophthalmus*)
  - Bobolink (*Dolichonyx oryzivorus*)
  - Buff-breasted sandpiper (*Calidris subruficollis*)
  - Franklin's gull (*Leucophaeus pipixcan*)
  - Lesser yellowlegs (*Tringa flavipes*)
  - Long-eared owl (*asio otus*)
  - Marbled godwit (*Limosa fedoa*)
  - Red-headed woodpecker (*Melanerpes erythrocephalus*)
  - Semipalmated sandpiper (*Calidris pusilla*)
  - Willet (*Tringa semipalmata*)
- Bald and Golden Eagle Protection Act
  - Bald eagle (*Haliaeetus leucocephalus*)

### 3.2.7 Soils

---

The USDA NRCS Web Soil Survey was used to gather baseline soils data at Elks River and 32<sup>nd</sup> Avenue South. The soils mapped within the study area consist of upland and hydric soils. The upland soils are concentrated in Grand Forks where the soils have been manipulated to form an urban area. The soils on the East Grand Forks side are primarily hydric soils that are less suitable for road construction and maintenance. Comparing the soils at Elks River and 32<sup>nd</sup> Avenue, there are no major differences between the soils that would hinder construction.

An extensive geotechnical field study will need to be conducted to understand the soil conditions present at the chosen bridge alternative before construction begins.

### 3.2.8 Trees

---

Boulevard trees border the residential streets of Grand Forks and patches of trees border the Red River. The largest area of native trees borders the Red River near Elks Drive. The future bridge roadway design will establish a new right-of-way that will impact any tree species within the proposed right-of-way.

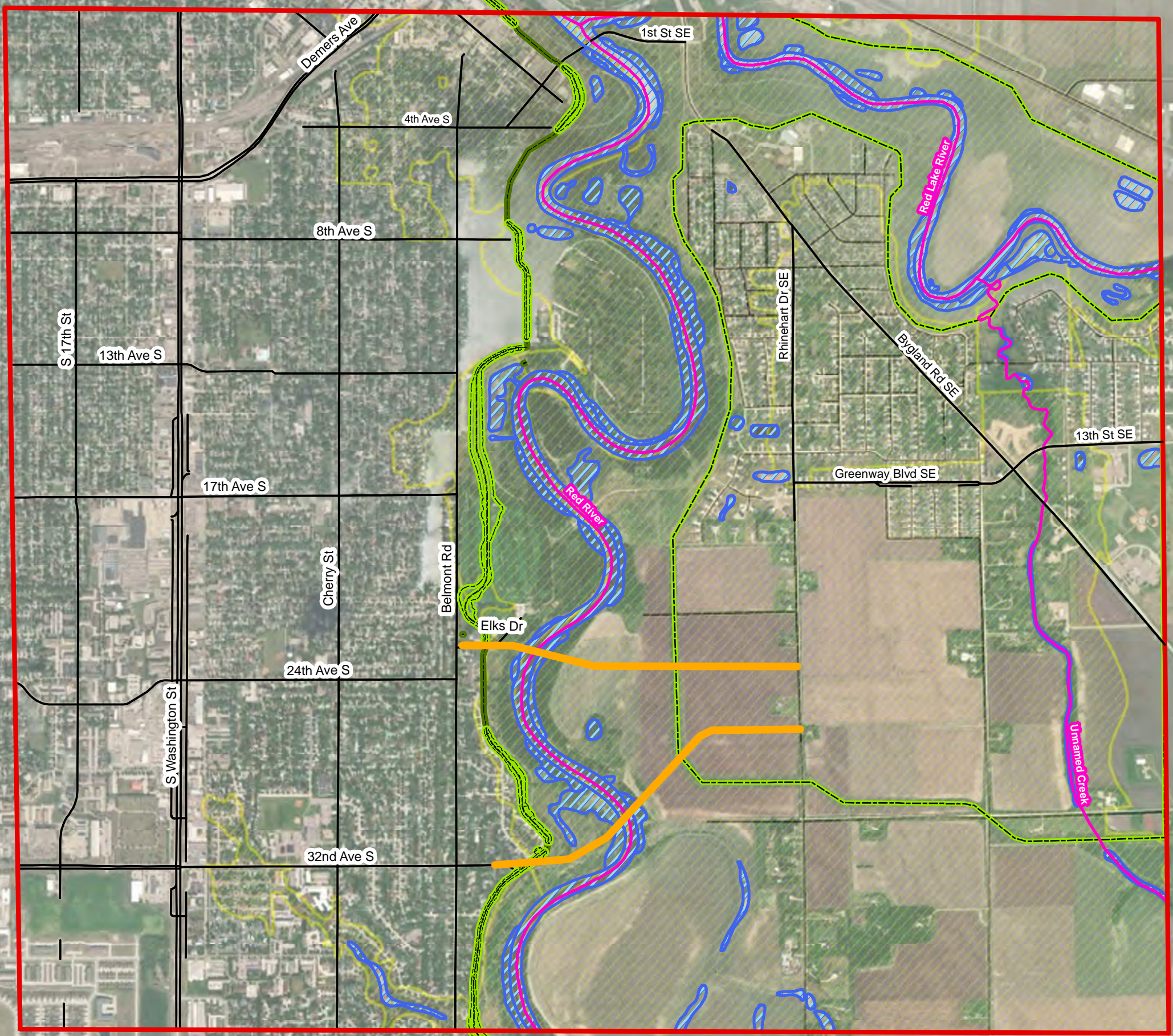
Any proposed tree removal should be conscious of bat roosting season. The U.S. Fish and Wildlife provides guidance for tree removal in order to avoid any impacts to bat species.

### Legend

- Study Area
- Bridge Alternatives
- Watercourse
- Earth Levee
- Flood Protection Wall
- National Wetland Inventory
- 100 YR Floodplain

Miles  
0 0.25 0.5

N



### 3.3 FARMLAND

Agricultural production is a significant industry for East Grand Forks and Polk County. The majority of the East Grand Forks area included in the study area is in agricultural production whereas the Grand Forks portion is primarily developed.

According to the USDA Natural Resources Conservation Service (NRCS), land in agricultural production within the East Grand Forks area is defined as prime farmland, prime farmland if drained, and prime farmland if protected from flooding or not frequently flooded during the growing season. The Grand Forks area was defined as not prime farmland. Potential impacts to prime farmland should be considered during the review of the bridge alternatives.

### 3.4 VISUAL

Impacts to the visual quality of the corridor should be considered as alternatives are developed for the corridor. Since all alternatives include a new bridge spanning the Red River, visual impacts will be reviewed closely for each alternative as part of the NEPA process.

### 3.5 AIR QUALITY

The existing factors that impact air quality within the study corridor now are vehicle-related air emissions mostly concentrated within Grand Forks that relate to traffic from the urban residential areas there. Due to the low density residential and agriculture land use in East Grand Forks, the vehicle-related air emissions are lower.

Construction of a new bridge will result in changes to traffic patterns and future traffic growth will result in an increase in traffic which will lead to an increase in carbon monoxide, carbon dioxide, and other vehicle-related air emissions in both Grand Forks and East Grand Forks. For screening purposes, increases in transportation air emissions will be assumed to scale with traffic volumes and will be evaluated in this manner.

During construction, best management practices (BMPs) should be used including the watering of dry, exposed soils to reduce dust in the surrounding area and maintaining construction entrances and exits to limit the tracking of soil onto the local roadways. The construction machinery on the site will be properly maintained to reduce odors such as exhaust from the diesel and gasoline powered machinery. Therefore, impacts from dust and odors during construction will be mitigated during construction.

### 3.6 NOISE

The existing factors that impact noise within the study corridor now are local roadway traffic. For the purposes of this analysis, traffic noise will be assumed to scale with traffic volumes and will be evaluated in this manner, with attention to potentially sensitive receptors such as residential areas and schools.

Noise will be generated temporarily during construction. The contractors will work in compliance with allowable working hours as established by the City of Grand Forks and the City of East Grand Forks ordinance. Factors affecting the noise level during construction will include the amount of construction that occurs

simultaneously, time of operation, and distance between construction equipment and receptors. The nearest sensitive receptors include the adjacent residential parcels in Grand Forks and East Grand Forks. Mitigation of short-term noise impacts should be managed through proper coordination and construction planning.

### 3.7 HAZARDOUS MATERIALS

There is potential for contaminated materials to be encountered during construction activities. The Minnesota Pollution Control Agency's "What's in my Neighborhood" and the U.S. Environmental Protection Agency's (US EPA) Region 8 Enforcement and Compliance History Online (ECHO) for North Dakota are searchable databases of known contaminated sites and environmental permits and registrations.

A Phase I Environmental Site Assessment (ESA) may be warranted to identify potential hazardous waste sites within the project area that may be disturbed during construction. If the results of the Phase I require further investigation, a Phase II environmental Site Assessment may also be needed to further evaluate the extent and composition of the contaminated materials within the project area.

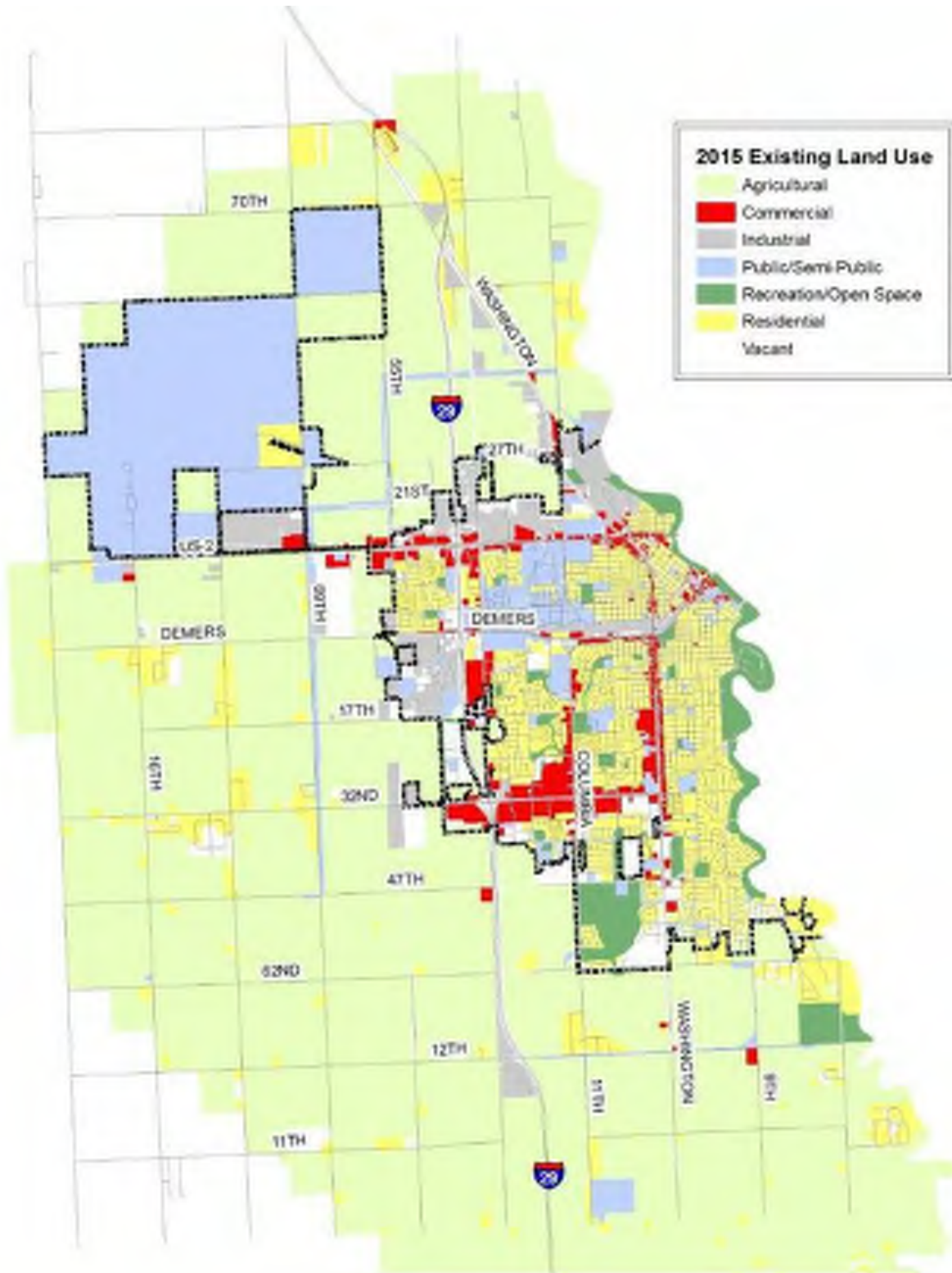
## 4. Land Use

---

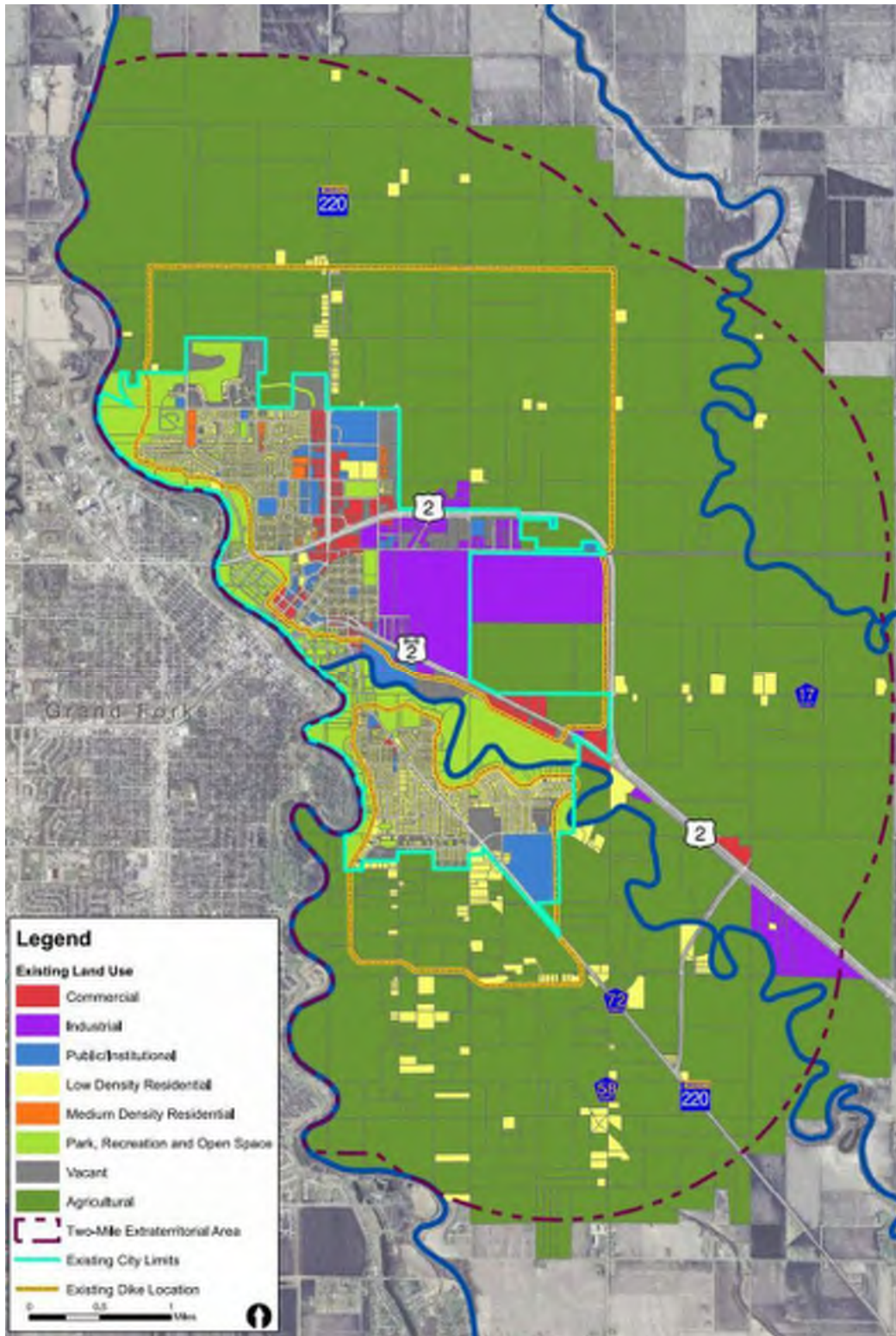
The study area encompasses a wide variety of existing land uses and density types. Both the City of Grand Forks and the City of East Grand Forks are in the process of evaluating existing land use and developing new plans. These plans are still in progress as of the preparation of this memo and no new maps are available at this time.

This section gives an overview of the land use plans prepared in 2015 and 2016. Updates to the plans will be incorporated into this study (traffic forecasting in particular) as relevant. Existing land use for each city is shown in **Figure 4-1** (Grand Forks) and **Figure 4-2** (East Grand Forks). Planned land use for each city is shown in **Figure 4-3** (Grand Forks) and **Figure 4-4** (East Grand Forks).

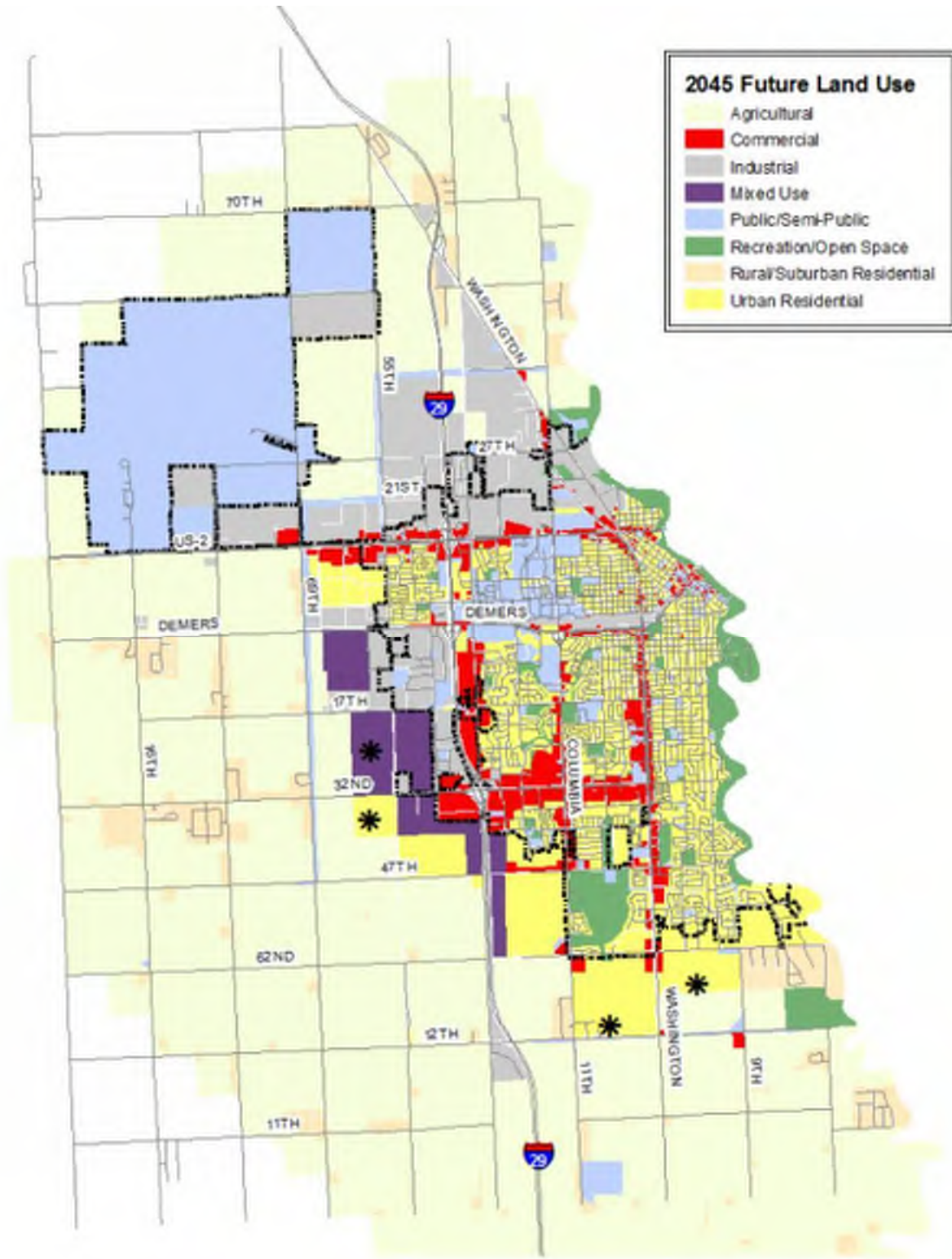




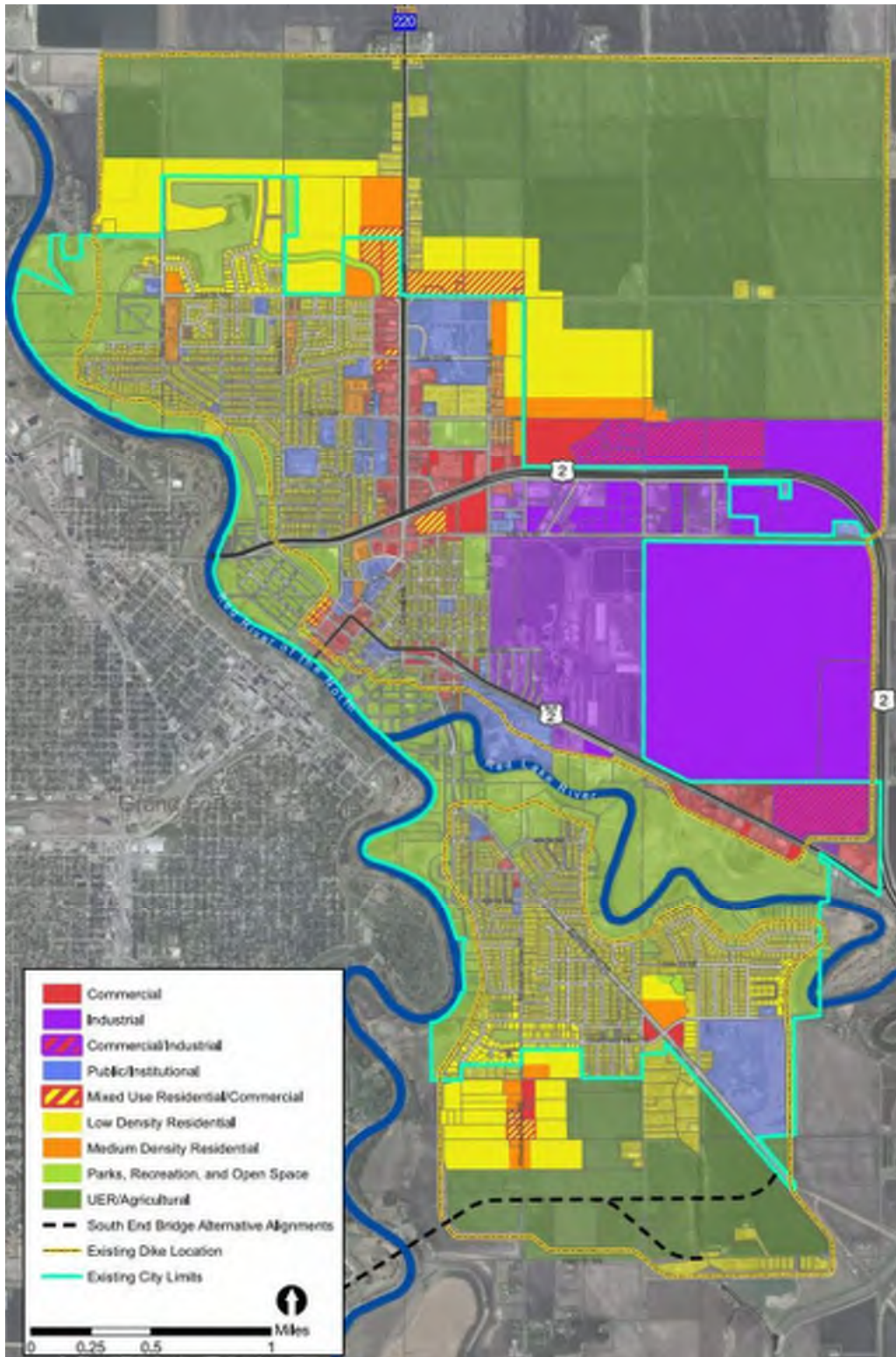
**Figure 4-1. Existing Land Use – Grand Forks**  
Source: 2045 Grand Forks Land Use Plan



**Figure 4-2. Existing Land Use – East Grand Forks**  
Source: East Grand Forks 2045 Land Use Plan



**Figure 4-3. Future (2045) Land Use Plan – Grand Forks**  
Source: 2045 Grand Forks Land Use Plan



**Figure 4-4. Future (2045) Land Use Plan – Grand Forks**

Source: East Grand Forks 2045 Land Use Plan



**MPO Staff Report**  
**Technical Advisory Committee:**  
April 14, 2021  
**MPO Executive Board:**  
April 21, 2021

**RECOMMENDED ACTION: Information on Earmarks.**

**TAC RECOMMENDED ACTION:**

Matter of the Earmarks.

**Background:** Congress has once again opened up the possibilities for earmarks. There are two different earmarking possibilities taking place right now. One is for appropriation bills for all the agencies' annual budgets. The second is specific to Transportation via the re-authorization bill that needs to be passed by the end of the federal fiscal year.

The House side is all in, although individual Congressperson(s) may decide not to participate. Senate side so far is only the Democrats that have indicated a willingness to earmark; Republicans have not yet determined whether they will. For re-authorization request, the named process is Member Designated Projects.

Possible projects the MPO is aware of:

- Joint Polk County and Grand Forks County request for "Merrifield Bridge"
- City of Grand Forks request for "42<sup>nd</sup> St Grade Separation"
- City of East Grand Forks request for "Neighborhood City to City Bridge"

NDDOT has indicated they are not submitting individual projects nor willing to endorse projects. MnDOT will monitor the situation and are not aware of projects that have been identified and being pursued for earmarks for the State Highway network

**Findings and Analysis:**

- Any earmark request will need some action from the MPO regarding its TIP and MTP.

**Support Materials:**

- NONE

**TABLE OF CONTENTS- UPDATE April, 2021**

**TRANSPORTATION PLAN UPDATE AND IMPLEMENTATION  
ACTIVITIES**

AREA	TASK	%	ORIGINAL COMPLETION DATE	PROJECTED COMPLETION DATE
Grand Forks Land Use Plan Update	First Land Use Sub-Committee was held on March 8th. Website is live: <a href="http://www.gf2050plan.com">www.gf2050plan.com</a> 2nd meeting is schedule for first Tuesday in May; Public Open House is scheduled May 11th.	35%	31-Dec-21	30-Mar-22
East Grand Forks Land Use Plan Update	Second survey and wiki mapping closed March 15th. Summaries of input have been drafted. Several "one on one" presentations have been done. <a href="http://www.egfplan.org">Www.egfplan.org</a>	60%	30-Jun-21	31-Dec-21
Future Bridge Traffic Impact Study	Ad Hoc Group met April 6th. Website established: <a href="http://www.forks2forksbridge.com">www.forks2forksbridge.com</a> Base conditions reoprt has been drafted and school safety surveys are being prepared.	19%	31-Dec-20	30-Dec-21
Pavement Management System Update	RFQ has been drafted and is being vetted during April meetings.	10%	31-Dec-21	30-Dec-21
Transit Development Program TDP	Draft agreement for #5307 funds is being vetted for approval in April; draft RFP has been provided to transit operators for review and comment and should be vetted for approval during May	8%	31-Mar-22	31-Mar-22
Aerial Photo	LiDAR has been captured; cloud cover has been preventing capturing the aerial photo - flight may take place any day now.	40%	30-Nov-21	30-Nov-21
Traffic Count Program	On-going; amended scope to add 3 new signal locations	90%	On-going	